



**ADDENDUM 1**

**July 29, 2020**

**ITB AP 56-20**

**OBSTRUCTION CLEARING AND PERIMETER FENCING IMPROVEMENTS  
AT BOB SIKES AIRPORT,  
Crestview Florida**

Please find attached the Document and information below, for the above referenced Addendum No. 1. This Addendum is hereby made a part of the Contract Documents and Specifications of the above referenced project. All other requirements of the original Contract Documents and Specifications shall remain effective in their respective order. The purpose of Addendum No. 1 is to set forth changes, add pre-bid meeting minutes, sign-in-sheet and/or additional information, as referenced herein.

**ACKNOWLEDGE RECEIPT OF THIS ADDENDUM (Pages 1 thru 4 and attachments) BY INSERTING ITS NUMBER AND DATE IN THE PROPOSAL FORM.**

**Note: The ITB Opening Date & Time remains unchanged.**

# **BOB SIKES AIRPORT**

**Crestview, FL**

## **ADDENDUM NO. 1**

For

### **OBSTRUCTION CLEARING AND PERIMETER FENCING IMPROVEMENTS**

RS&H No. 201-0251-013  
FDOT No. 1: 425616-4-94-01  
FDOT No. 2: 425618-6-94-01



RS&H, Inc.  
Mobile, AL

July 28, 2020

## ADDENDUM NO. 1

This Addendum is hereby made a part of the Contract Documents and Specifications of the above referenced project. All other requirements of the original Contract Documents and Specifications shall remain effective in their respective order.

**ACKNOWLEDGE RECEIPT OF THIS ADDENDUM (Pages 1 thru 4 and attachments) BY INSERTING ITS NUMBER AND DATE IN THE PROPOSAL FORM.**

### GENERAL

1. The BID DATE, BID TIME, and BID LOCATION are **UNCHANGED** by this addendum.
2. A project pre-bid conference, open to all interested bidders, was held at the Bob Sikes Airport administration office on July 16, 2020. The pre-bid meeting minutes and the meeting sign-in sheet are included in this addendum for reference.

### MODIFICATIONS TO BID FORM

3. Page BF-7, BF-8, and BF-9, **REPLACE** quantities for pay items in Base Bid and Bid Alternate 1.

### MODIFICATIONS TO THE PLANS

4. Sheet C001, INDEX OF DRAWINGS AND SUMMARY OF CONTRACT QUANTITIES, multiple pay item quantities changed to accommodate moving Area 1 from Base Bid to Bid Alternate 1.
5. Sheet C003, CONTRACT LAYOUT PLAN, General Contract Note 3 revised to prohibit burning of vegetation cleared on project on Airport property.
6. Sheet C004, CONSTRUCTION PHASING PLAN, Phase 1 notes revised to remove Area 1.
7. Sheet C004, CONSTRUCTION PHASING PLAN, Phase 3A duration changed to be 3 consecutive calendar days of night work.
8. Sheet C004, CONSTRUCTION PHASING PLAN, Phase 4 notes revised to add Area 1.
9. Sheet C004, CONSTRUCTION PHASING PLAN, Area 1 hatch in plan view revised to indicate Phase 4.
10. Sheet C100, CLEARING AND FENCING PLANS – KEY MAP, Area 1 hatch revised to indicate Bid Alternate 1.
11. Sheet C101, CLEARING AND FENCING PLANS (SHEET 1 OF 7), Area 1 revised to be in Bid Alternate 1.
12. Sheet C111 CLEARING AND FENCING DETAILS, Clearing and Grubbing Note 4 revised to prohibit burning of cleared vegetation on Airport property.

### MODIFICATIONS TO THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP)

13. Section 3.5.1 LOCATION OF STOCKPILED CONSTRUCTION MATERIALS on Page 8, revised to prohibit burning of cleared vegetation on Airport property.

## **RESPONSES TO QUESTIONS ASKED AT PRE-BID CONFERENCE**

1. **Question:** Is there an established date that construction has to be complete as part of the grant requirements?  
**Answer:** No, there is no set date that the construction must be complete for the grant.
2. **Question:** What is the timeline for contract award and construction?  
**Answer:** While no guarantees can be made regarding timelines, it is anticipated that a notice to proceed for construction could be issued within two months of the bid opening.
3. **Question:** What is the construction budget?  
**Answer:** A firm budget has not been established and is dependent on possible alternate funding sources.
4. **Question:** Are there any limitations on work time (days/times)?  
**Answer:** The Airport can accommodate work in all construction phases (with exception of work in Phase 3A, requiring runway closure) seven days a week at any hour of the day.
5. **Question:** Which Davis-Bacon Wage Rates will apply?  
**Answer:** The current Highway wage rates for Okaloosa County, FL (FL20200179) will apply.
6. **Question:** How will fencing replacement along existing fenceline occur if the perimeter security must remain secure at all times and the proposed fencing fabric cannot be installed until the concrete for the line posts has to be cured?  
**Answer:** In areas where the contract documents indicate to replace existing fencing in the same location, the proposed fenceline can be offset 3-feet from the existing fenceline in order to maintain Airport security.

## **RESPONSES TO QUESTIONS SUBMITTED IN WRITING TO OKALOOSA COUNTY**

*None received.*

## **LIST OF ATTACHMENTS**

1. Pre-Bid Meeting Minutes
2. Pre-Bid Meeting Sign-In Sheet
3. Bid Form
4. Plan Sheet C001, INDEX OF DRAWINGS AND SUMMARY OF CONTRACT QUANTITIES
5. Plan Sheet C003, CONTRACT LAYOUT PLAN
6. Plan Sheet C004, CONSTRUCTION PHASING PLAN
7. Plan Sheet C100, CLEARING AND FENCING PLANS – KEY MAP
8. Plan Sheet C101, CLEARING AND FENCING PLANS (SHEET 1 OF 7)
9. Plan Sheet C111, CLEARING AND FENCING DETAILS
10. Construction Safety and Phasing Plan (CSPP)



**PRE-BID MEETING MINUTES  
OBSTRUCTION CLEARING AND  
FENCING IMPROVEMENTS  
BOB SIKES AIRPORT  
RS&H PROJECT NO. 201.0251.013  
FDOT NO (1): 425616-4-94-01 (G1H77)  
FDOT NO (2): 425618-6-94-01 (G1799)  
July 16, 2020  
10:00 AM (Central Time)**

1. **Introduction of Personnel and Sign In:** Chad Rogers explained safety protocols, requiring all attendees to wear a face covering at all times during the Pre-Bid Conference. Chad Rogers stated that the project will be funded primarily from two different FDOT grants. All attendees introduced themselves, providing name and organization.

2. **Project Description:** Matt Thomason gave the following general description of the project scope:

Project consists of clearing/grubbing/seeding areas of on Airport property that contain obstructions (trees) penetrating various Airport TERPs surfaces in critical locations. Areas to be cleared in this project have been prioritized by breaking them out into Base Bid versus Bid Alternate areas.

Project additionally consists of the replacement and/or relocation of existing security fencing on two different parts of the Airport with new security fencing containing a wildlife deterrent skirt.

Project also includes the regrading of approximately 0.5 acres of turf inside the Runway 17 runway safety area in order to meet FAA runway safety area grading requirements.

3. **Plans and Specifications:**  
Contract documents are available by electronic delivery only. To obtain contract documents, visit:  
Okaloosa County Purchasing Current Solicitations & Public Notices  
<http://www.co.okaloosa.fl.us/purchasing/current-solicitations>  
Document Number: ITB AP 56-20

4. **Bid Date, Time, and Location:** 3:15 PM (local time) on August 5, 2020  
Sealed bids will be received by the Okaloosa Board of County Commissioners, 5479A Old Bethel Rd, Crestview, FL 32536, until 3:15 PM (local time), on the bid date, at which time and place all bids will be publicly opened and read aloud. Bids must be in the possession of the County prior to bid time on the bid date.

The bid, with blue-ink original signatures, and two (2) additional copies are to be submitted in a sealed envelope and the sealed envelope shall be marked as follows:  
BID ENCLOSED – ITB AP 56-20 CEW OBSTRUCTION CLEARING AND FENCING IMPROVEMENTS

5. **Form of Bid:** Unit Price

6. **Disadvantaged Business Enterprise:** No specific goal is established. DBE participation is encouraged, but not required for submittal of bids for this project. Chad Rogers noted that any DBE participation on this project will need to be documented for County tracking purposes.
7. **Buy American Provision:** Mandated compliance.
8. **Contract wage rates:** Certified Payrolls in accordance with Davis-Bacon Act are required.
9. **Bonds & Insurance:** 5% Bid Bond required, 100% Performance and Payment Bond will be required of the successful bidder. Bid prices shall be held for up to **120** calendar days.
10. **Permits:** Contractor shall be responsible for obtaining all necessary Permits.
11. **Other Requirements for Bidders:** Matt Thomason encouraged all interested bidders to refer to Project Manual for all bidding requirements.
12. **Schedule:**

Matt Thomason gave a description of the project construction phasing from sheet C004 of the project plan set.

	<b>Base Bid</b>
<b>Procurement/Mobilization (Phase 0)</b>	30 (Max)
<b>Phase 1</b>	24
<b>Phase 2</b>	22
<b>Phase 3/3A</b>	14
<b>Total BASE BID (Including Procurement)</b>	90
	<b>Bid Alternates</b>
<b>Phase 4</b>	30
<b>Phase 5</b>	30
<b>Total BID ALTERNATES</b>	60

Chad Rogers stated that clearing and grubbing of Area 1 on the southwest corner of the Airport will be moved from the Base Bid to Bid Alternate 1 as part of Addendum 01.

13. **Questions from Bidders to Date**  
*There were not any questions received by any bidders prior to the Pre-Bid Conference.*

14. **Questions from Attendees:**  
*The following questions were received by attendees at the Pre-Bid Conference. The responses provided were not explicitly provided in the Pre-Bid Conference but are included in the minutes for reference. These questions and responses are also included in the Addendum 01 summary.*

- Question: Is there an established date that construction has to be complete as part of the grant requirements?
  - Response: No, there is no set date that the construction must be complete for the grant.

- Question: What is the timeline for contract award and construction?
  - Response: While no guarantees can be made regarding timelines, it is anticipated that a notice to proceed for construction could be issued within two months of the bid opening.
- Question: What is the construction budget?
  - Response: A firm budget has not been established and is dependent on possible alternate funding sources.
- Question: Are there any limitations on work time (days/times)?
  - Response: The Airport can accommodate work in all construction phases (with exception of work in Phase 3A, requiring runway closure) seven days a week at any hour of the day.
- Question: Which Davis-Bacon Wage Rates will apply?
  - Response: The current Highway wage rates for Okaloosa County, FL (FL20200179) will apply.
- Question: How will fencing replacement along existing fenceline occur if the perimeter security must remain secure at all times and the proposed fencing fabric cannot be installed until the concrete for the line posts has to be cured?
  - Response: In areas where the contract documents indicate to replace existing fencing in the same location, the proposed fenceline can be offset 3-feet from the existing fenceline in order to maintain Airport security.

15. **Questions after Pre-Bid Conference:**

All questions regarding Bid Documents shall be submitted in writing to the Okaloosa County Purchasing Office. The deadline for inquiries is **Wednesday, July 22, 2020 at 3:30 PM** (Central Time). Inquiries submitted after this deadline will not receive responses. All timely inquiries will be responded by addendum. No telephone inquiries will be accepted.

16. **Procedure for Addendums:**

All addendums will be issued electronically in the same delivery method as the Bid Documents.

Matt Thomason encouraged interested bidders to monitor the Okaloosa County Purchasing website for addendums on this project. Jessica Darr noted that interested bidders can register on Demandstar or Bidnet for addendum notifications for this project.

17. **Other Issues:**

- Safety
  - Matt Thomason emphasized the need for Foreign Object Debris (FOD) control. Windblown debris like dust and trash from the construction activity has the potential to damage aircraft and impair the overall safety of the Airport and construction site(s).
  - Chad Rogers discussed the need for the selected contractor to have an adverse weather plan, which would include securing construction equipment, materials, and debris if a tropical weather system is anticipated.

- Work in Phase 3A will require the temporary closure of Runway 17-35. The Airport will provide the required lighted X's for this closure. It will be the Contractor's responsibility to keep the generators on these X's fueled and maintained.
- Security
  - The selected Contractor will be required to provide the Airport a list of all personnel who will be working on the project. No background checks will be performed, but all individuals working on the project must be listed.
  - The Airport will provide access gate cards to key personnel on the project for access to the secure Air Operations Area (AOA).
- Coordination w/Owner & Engineer
  - The Contractor and Airport will meet on a regular basis (weekly or biweekly) during construction to discuss project progress. These meetings will likely occur at the Airport.

18. **Open Discussion**

*The following items were discussed in the Pre-Bid Conference, outside of the established agenda.*

- Chad Rogers discussed the following options available to the selected contractor for debris removal.
  - Haul off and legally disposed of off Airport property.
  - Mulch and dispose of on-Airport property at locations to be established by the Owner prior to the start of construction.
  - Burning of debris on Airport property.
    - **Note: After the Pre-Bid Conference, the Owner decided to remove this option from the project. This change is reflected in Addendum 01. Any further questions or discussion from the Pre-Bid Conference regarding burning of debris on Airport property has been omitted from the minutes for clarity.**
- Chad Rogers emphasized that Bidders need to be familiar with the insurance requirements in the up-front specifications provided in the Bid Documents.
- Matt Thomason stated that the selected Contractor will be allowed to store construction equipment in areas of the project that they are actively working in, with the exception of work areas inside the Runway 17-35 RSA (Phase 3A). The Contractor will not be required to move all equipment back to the designated staging areas at the end of each work day in areas they are actively working in.

19. **Meeting Adjourned**

The meeting was adjourned at approximately 11:00 AM (CST). A site visit of the various project areas was conducted for interested attendees immediately following the Pre-Bid Conference.



PRE-BID CONFERENCE SIGN-IN SHEET

July 16, 2020 at 10:00 a.m. Central  
ITB AP 56-20

Tree Clearing & Security Fence  
Bob Sikes Airport

NAME	REPRESENTING	TELEPHONE	E-MAIL ADDRESS
Tracy Stage	Okaloosa County Airports	850-651-7160	tstage@myokaloosa.com
Chad Rogers	Okaloosa County Airports	850-651-7160 x01055	crogers@myokaloosa.com
RS Ray Beasley	Okaloosa County Airports	850-380-0659	rbeasley@myokaloosa.com
Tony Peterson	Okaloosa County Airports	850-826-0001	apeterson@myokaloosa.com
Matt Thomason	RS&H	850-554-7016	matthew.thomason@rsandh.com
Gattin Rossi	AEF	850-517-4446	gross@aeconcrete.com
Blake Casaway	Amecor Eagle Force	850 887-8104	bcasaway@amecorforce.com
Edwin Collins	Collins and Son	850 244 8093	Edwin@CollinsandSon.com
Royce Fisher	Merchandising	318-201-3521	Royce@MSU.WORK JOHN@BELME.COM
Ben Starnes	Stary Industries	863-581-0215	Ben@StaryIndustries.com

PRE-BID CONFERENCE SIGN-IN SHEET

July 16, 2020 at 10:00 a.m. Central  
ITB AP56-20

Tree Clearing & Security Fence  
Bob Sikes Airport

NAME	REPRESENTING	TELEPHONE	E-MAIL ADDRESS
Perry Bell	ECF Inc.	(850) 892-0291	perrygbell@yahoo.com
David Wilson	ECF Inc.	850-401-9085	davidwgcf@gmail.com
Devin Jones	WLF Land	850 978.2142	wlfcompany@gmail.com
Scott Blaker	Warwick	832-448-5806	SBlaker@WarwickConstruction.com
Jose Enriquez	MEDIA INT. GROUP	787-365-2154	JENRIQUEZ@MEDIATELECOM.COM
JOSÉ TORRES	11	352-214-9506	JTORRES@MEDIATELECOM.COM
Greg Bondurat	Green Energy Contracting	850-305-7346	gregb@grecontracting.net

**BID SCHEDULE - UNIT PRICES**

**CONTRACTOR:** \_\_\_\_\_ **DATE:** \_\_\_\_\_

**AIRPORT NAME:** Bob Sikes Airport  
**PROJECT DESCRIPTION:** Obstruction Clearing and Fencing Improvements

**BASE BID SCHEDULE**

Quote Item No.	Item No.	Item Description & Unit Price In Words	Unit	Estimated Quantity	Unit Price	Total Amt./ Item
1	C-105-6.1	<u>Mobilization</u> _____ dollars and _____ cents	LS	1		
2	C-102-5.1	<u>Installation and Removal of Silt Fence</u> _____ dollars and _____ cents	LF	11,000		
3	P-102-10.1	<u>Safety and Security</u> _____ dollars and _____ cents	LS	1		
4	P-104-5.1	<u>Project Survey and Stakeout – Base Bid</u> _____ dollars and _____ cents	LS	1		
5	P-151-4.1	<u>Clearing and Grubbing</u> _____ dollars and _____ cents	AC	14.5		
6	P-152-4.1	<u>Unclassified Excavation</u> _____ dollars and _____ cents	CY	850		

7	F-162-5.1	<u>Permanent Chain-Link Fence with Wildlife Skirt</u> _____ dollars and _____ cents	LF	2,800		
8	F-162-5.2	<u>Existing Fence Removal</u> _____ dollars and _____ cents	LF	2,700		
9	T-901-5.1	<u>Seeding</u> _____ dollars and _____ cents	AC	16.0		
10	T-908-5.1	<u>Mulching</u> _____ dollars and _____ cents	AC	16.0		

**BID ALT 1 SCHEDULE**

Quote Item No.	Item No.	Item Description & Unit Price In Words	Unit	Estimated Quantity	Unit Price	Total Amt./ Item
11	C-102-5.1	<u>Installation and Removal of Silt Fence</u> _____ dollars and _____ cents	LF	8,500		
12	P-104-5.2	<u>Project Survey and Stakeout – Bid Alt 1</u> _____ dollars and _____ cents	LS	1		
13	P-151-4.1	<u>Clearing and Grubbing</u> _____ dollars and _____ cents	AC	19.0		
14	T-901-5.1	<u>Seeding</u> _____ dollars and _____ cents	AC	19.0		

15	T-908-5.1	<u>Mulching</u> _____ dollars and _____ cents	AC	19.0		
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**BID ALT 2 SCHEDULE**

Quote Item No.	Item No.	Item Description & Unit Price In Words	Unit	Estimated Quantity	Unit Price	Total Amt./ Item
16	C-102-5.1	<u>Installation and Removal of Silt Fence</u> _____ dollars and _____ cents	LF	4,000		
17	P-104-5.3	<u>Project Survey and Stakeout – Bid Alt 2</u> _____ dollars and _____ cents	LS	1		
18	P-151-4.1	<u>Clearing and Grubbing</u> _____ dollars and _____ cents	AC	8.5		
19	T-901-5.1	<u>Seeding</u> _____ dollars and _____ cents	AC	8.5		
20	T-908-5.1	<u>Mulching</u> _____ dollars and _____ cents	AC	8.5		

**BID ALT 3 SCHEDULE**

Quote Item No.	Item No.	Item Description & Unit Price In Words	Unit	Estimated Quantity	Unit Price	Total Amt./ Item
21	C-102-5.1	<u>Installation and Removal of Silt Fence</u> _____ dollars and _____ cents	LF	3,000		
22	P-104-5.4	<u>Project Survey and Stakeout – Bid Alt 3</u> _____ dollars and _____ cents	LS	1		
23	P-151-4.1	<u>Clearing and Grubbing</u> _____ dollars and _____ cents	AC	4.5		
24	T-901-5.1	<u>Seeding</u> _____ dollars and _____ cents	AC	4.5		
25	T-908-5.1	<u>Mulching</u> _____ dollars and _____ cents	AC	4.5		

**FOR ALL WORK REQUIRED IN ACCORDANCE WITH THE CONSTRUCTION DRAWINGS, SPECIFICATIONS AND OTHER CONTRACT DOCUMENTS, INCLUDING ALL COSTS RELATED TO THE WORK, AND ANY REQUIRED PERMITS, TAXES, BONDS AND INSURANCE, THE UNDERSIGNED SUBMITS A TOTAL BID AMOUNT OF:**

**TOTAL BASE BID (amount in words):**

\_\_\_\_\_ Dollars and  
\_\_\_\_\_ cents  
  
(\$ \_\_\_\_\_)  
**(amount in numbers)**

**TOTAL ALT 1 BID (amount in words):**

\_\_\_\_\_ Dollars and  
\_\_\_\_\_ cents  
  
(\$ \_\_\_\_\_)  
**(amount in numbers)**

**TOTAL ALT 2 BID (amount in words):**

\_\_\_\_\_ Dollars and  
\_\_\_\_\_ cents  
  
(\$ \_\_\_\_\_)  
**(amount in numbers)**

**TOTAL ALT 3 BID (amount in words):**

\_\_\_\_\_ Dollars and  
\_\_\_\_\_ cents  
  
(\$ \_\_\_\_\_)  
**(amount in numbers)**

The Contractor represents that it has examined the site of the Work and informed itself fully in regard to all conditions

pertaining to the place where the work is to be done; that it has examined the plans and specifications for the work and other Contract Documents relative thereto and has read all of the Addenda furnished prior to the opening of the Bids, as acknowledged below; and that it has otherwise fully informed itself regarding the nature, extent, scope and details of the Work to be performed.

If provided with a Notice of Intent to Award the Contract by the Owner, the Contractor shall execute and deliver to the Owner all of the documents required by the Contract Documents, including but not limited to, the Addendum to the Agreement and the Performance and Payment Bonds in the form contained in the Contract Documents, furnish the required evidence of the specified insurance coverages, furnish all necessary permits, license, materials, equipment, machinery, maintenance, tools, apparatus, means of transportation and labor necessary to complete the Work.

Dated and signed at \_\_\_\_\_, \_\_\_\_\_, this \_\_\_\_\_ day of \_\_\_\_\_, 2020.

\_\_\_\_\_  
(Name of Bidder)

\_\_\_\_\_  
(Authorized Signature)

\_\_\_\_\_  
(Title)

\_\_\_\_\_  
(Mailing Address)

\_\_\_\_\_  
(City, State, Zip)

\_\_\_\_\_  
(Federal ID No. or SS No.)



INDEX OF DRAWINGS		
DRAWING NUMBER	SHEET NUMBER	SHEET TITLE
<b>GENERAL</b>		
C000	1	COVER SHEET
C001	2	INDEX OF DRAWINGS AND SUMMARY OF QUANTITIES
C002	3	SAFETY AND SECURITY NOTES AND DETAILS
C003	4	CONTRACT LAYOUT PLAN
C004	5	CONSTRUCTION PHASING PLAN
<b>CIVIL</b>		
C100	6	CLEARING AND FENCING PLANS - KEY MAP
C101	7	CLEARING AND FENCING PLANS (1 OF 7)
C102	8	CLEARING AND FENCING PLANS (2 OF 7)
C103	9	CLEARING AND FENCING PLANS (3 OF 7)
C104	10	CLEARING AND FENCING PLANS (4 OF 7)
C105	11	CLEARING AND FENCING PLANS (5 OF 7)
C106	12	CLEARING AND FENCING PLANS (6 OF 7)
C107	13	CLEARING AND FENCING PLANS (7 OF 7)
C111	14	CLEARING AND FENCING DETAILS
C201	15	EROSION AND SEDIMENTATION CONTROL NOTES AND DETAILS

SUMMARY OF QUANTITIES							
NO.	ITEM NUMBER	ITEM DESCRIPTION	UNIT	BID QUANTITY	CHANGE ORDER	FINAL QUANTITY	
<b>BASE BID - AREA 2, AREA 3, AND AREA 4</b>							
1	C-105-6.1	MOBILIZATION	LS	1			
2	C-102-5.1	INSTALLATION AND REMOVAL OF SILT FENCE	LF	11,000			
3	P-102-10.1	SAFETY AND SECURITY	LS	1			
4	P-104-5.1	PROJECT SURVEY AND STAKEOUT - BASE BID	LS	1			
5	P-151-4.1	CLEARING AND GRUBBING	AC	14.5			
6	P-152-4.1	UNCLASSIFIED EXCAVATION	CY	850			
7	F-162-5.1	PERMANENT CHAIN-LINK FENCE W/ WILDLIFE SKIRT	LF	2,800			
8	F-162-5.2	EXISTING FENCE REMOVAL	LF	2,700			
10	T-901-5.1	SEEDING	AC	16.0			
11	T-908-5.1	MULCHING	AC	16.0			
<b>BID ALTERNATE 1 - AREA 1 AND AREA 5</b>							
12	C-102-5.1	INSTALLATION AND REMOVAL OF SILT FENCE	LF	8,500			
13	P-104-5.2	PROJECT SURVEY AND STAKEOUT - BID ALT 1	LS	1			
14	P-151-4.1	CLEARING AND GRUBBING	AC	19.0			
15	T-901-5.1	SEEDING	AC	19.0			
16	T-908-5.1	MULCHING	AC	19.0			
<b>BID ALTERNATE 2 - AREA 6</b>							
17	C-102-5.1	INSTALLATION AND REMOVAL OF SILT FENCE	LF	4,000			
18	P-104-5.3	PROJECT SURVEY AND STAKEOUT - BID ALT 2	LS	1			
19	P-151-4.1	CLEARING AND GRUBBING	AC	8.5			
20	T-901-5.1	SEEDING	AC	8.5			
21	T-908-5.1	MULCHING	AC	8.5			
<b>BID ALTERNATE 3 - AREA 7</b>							
22	C-102-5.1	INSTALLATION AND REMOVAL OF SILT FENCE	LF	3,000			
23	P-104-5.4	PROJECT SURVEY AND STAKEOUT - BID ALT 3	LS	1			
24	P-151-4.1	CLEARING AND GRUBBING	AC	4.5			
25	T-901-5.1	SEEDING	AC	4.5			
26	T-908-5.1	MULCHING	AC	4.5			

## ABBREVIATIONS

ABC - AGGREGATE BASE COURSE  
AC/ACP - ASPHALTIC CONCRETE PAVEMENT  
ACI - AMERICAN CONCRETE INSTITUTE  
ADG - AIRPLANE DESIGN GROUP  
AIP - AIRPORT IMPROVEMENT PROGRAM  
AOA - AIRCRAFT OPERATIONS AREA  
API - AMERICAN PETROLEUM INSTITUTE  
APPROX - APPROXIMATE  
ASIG - AIRCRAFT SERVICE INTERNATIONAL GROUP  
ASR - AIRPORT SURVEILLANCE RADAR  
ASTM - AMERICAN SOCIETY FOR TESTING AND MATERIALS  
AUG - AUGER  
AWG - AMERICAN WIRE GAUGE  
B - BASELINE  
BF - BLIND FLANGE  
BFV - BUTTERFLY VALVE  
BLDG - BUILDING  
BO - BOTTOM OF  
BOSC - BOTTOM OF SECONDARY CONTAINMENT  
BOP - BOTTOM OF PIPE  
BOD - BOTTOM OF DUCT  
BODWP - BOTTOM OF DOUBLE WALL PIPE  
BOTT - BOTTOM  
CLC - CENTERLINE  
C - CONDUIT  
CD-ROM - COMPACT DISK-READ ONLY MEMORY  
CBR - CALIFORNIA BEARING RATIO  
CKT - CIRCUIT  
CLR - CLEARANCE  
COM - COMMUNICATION  
CONC - CONCRETE  
CONN - CONNECTION  
CONT - CONTINUOUS  
CS - CARBON STEEL

CS - CARBON STEEL  
CSSO - CONTRACTOR SECURITY AND SECURITY OFFICER  
CVV - CONTROL VALVE VAULT  
DB&B - DOUBLE BLOCK & BLEED  
DBL - DOUBLE  
DET - DETAIL  
DIP - DUCTILE IRON PIPE  
DIA - DIAMETER  
DIAG - DIAGONAL  
DWG - DRAWING  
DWJFP - DOUBLE WALL JET FUEL PIPE  
E - EASTING  
EA - EACH  
ECC - ECCENTRIC  
EFSO - EMERGENCY FUEL SHUT-OFF SYSTEM  
EG - EXISTING GROUND  
ELEV - ELEVATION  
ELEC - ELECTRICAL  
ELL - ELBOW  
EMT - ELECTRICAL MECHANICAL TUBING  
EOP - EDGE OF PAVEMENT  
EX/EXST/EXIST - EXISTING  
FAA - FEDERAL AVIATION ADMINISTRATION  
FBI - FEDERAL BUREAU OF INVESTIGATIONS  
FD - FIRE DEPARTMENT  
FDEP - FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION  
FDOT - FLORIDA DEPARTMENT OF TRANSPORTATION  
FF - FINISHED FLOOR  
FH - FIRE HYDRANT  
FLG - FLANGE  
FLR - FLOOR  
FM - FORCE MAIN  
FM - FLOW METER  
FNPT - FEMALE NATIONAL FIRE THREAD  
FO - FIBER OPTIC  
FOD - FOREIGN OBJECT DEBRIS

FAR - FEDERAL AVIATION REGULATIONS  
FW - FIRE WATER  
GALV - GALVANIZED  
GND - GROUND  
GNE - GROUNDWATER NOT ENCOUNTERED  
GRS - GALVANIZED RIGID STEEL  
GSE - GROUND SERVICE EQUIPMENT  
GV - GAS VALVE  
GWL - GROUND WATER LEVEL  
HORT - HORIZONTAL  
HP - HYDRANT PIT  
HPV - HIGH POINT VENT  
IIC - NUMBER OF CONDUCTORS/CONDUCTOR  
ID - IDENTIFICATION  
I.D. - INNER DIAMETER  
IE/INV - INVERT ELEVATION / INVERT  
I.F. - INSULATING FLANGE  
ISO - ISOLATION  
IVP - ISOLATION VALVE PIT  
JF - JET FUEL  
JFP - JET FUEL PIPE  
KIP - 1,000 LBS  
KV - KILOVOLT  
L - LIGHTING  
LB - POUND  
LCD - LIQUID-CRYSTAL DISPLAY  
LED - LIGHT-EMITTING DIODE  
LEO - LAW ENFORCEMENT OFFICER  
LF - LINEAR FEET  
LPD - LOW POINT DRAIN  
LT - LEFT  
MAX - MAXIMUM  
MECH - MECHANICAL  
MH - MANHOLE

MIN - MINIMUM  
MNPT - MALE NATIONAL PIPE THREAD  
MON - MONITORING (WELL)  
MSL - MEAN SEA LEVEL  
N - NOTHING OR NO. OF BLOWS  
NAD - NORTH AMERICAN DATUM  
NAVAIDS - NAVIGATIONAL AIDS  
NEC - NATIONAL ELECTRIC CODE  
NFPA - NATIONAL FIRE PROTECTION ASSOCIATION  
NGVD - NATIONAL GEODETIC VERTICAL DATUM  
NO - NUMBER  
NOTAM - NOTICE TO AIRMEN  
NPT - NATIONAL PIPE THREAD  
NTS - NOT TO SCALE  
OC - ON CENTER  
OD - OUTER DIAMETER  
OFA - OBJECT FREE AREA  
O/S - OUTSIDE  
OZ - OUNCE  
PAVT - PAVEMENT  
PC - POINT OF CURVATURE  
PCC - PORTLAND CEMENT CONCRETE  
PERF - PERFORATED  
PF - PLIDCO FLANGE  
PG - PRESSURE GAUGE  
PG - PROPOSED GRADE  
PK - PEAK  
PKG - PARKING  
PL - PLATE  
PNL-CKT - PANEL CIRCUIT  
PS - PIPE SUPPORT  
PSF - POUNDS PER SQUARE FOOT  
PSI - POUNDS PER SQUARE INCH  
PT - POINT OF TANGENCY  
PVC - POLYVINYL CHLORIDE

PVI - POINT OF VERTICAL INTERSECTION  
RCP - REINFORCED CONCRETE PIPE  
RD - ROOF DRAIN  
RED - REDUCER  
REF - REFERENCE  
REINF - REINFORCED  
REQ/REQ'D - REQUIRED  
RGL - RUNWAY GUARD LIGHTS  
RF - RAISED FACE  
RIM - RIM ELEVATION  
RPR - RESIDENT PROJECT REPRESENTATIVE  
RPZ - RUNWAY PROTECTION ZONE  
RSA - RUNWAY SAFETY AREA  
RT - RIGHT  
RVZ-F - RUNWAY VISUAL ZONE - F  
RW - RUNWAY  
RWL - RAIN WATER LEADER  
S/SAN - SANITARY  
SC - SECONDARY CONTAINMENT  
SCH/SCHED - SCHEDULE  
SD - STORM DRAIN  
SHT - SHEET  
SIDA - SECURITY IDENTIFICATION DISPLAY AREA  
SF - SQUARE FOOT  
SF/GAL - SQUARE FEET PER GALLON  
SP - SINGLE PIPE  
SP-SC - SINGLE PIPE / SECONDARY CONTAINMENT  
SPT - STANDARD PENETRATION TEST  
SS - STAINLESS STEEL  
STA - STATION  
PS - PIPE SUPPORT  
STD - STANDARD  
STL - STEEL  
STRUCT - STRUCTURE  
SWJFP - SINGLE WALL JET FUEL PIPELINE  
SW - SINGLE WALL

TBR - TO BE REMOVED  
TDZ - TOUCHDOWN ZONE  
TEL - TELEPHONE  
TEMP - TEMPORARY  
THD - THREADED  
THK - THICK  
TL - TAXILANE  
T.O. - TOP OF  
TOC - TOP OF CONCRETE  
TOD - TOP OF DUCT  
TOFA - TAXIWAY OBJECT FREE AREA  
TSA - TAXIWAY SAFETY AREA  
TSP - TWISTED SHIELDED PAIR  
TW - TAXIWAY  
TYP - TYPICAL  
UG - UNDERDRAIN  
UMH - UNIDENTIFIED MANHOLE  
U.N.O. - UNLESS NOTED OTHERWISE  
VA - VOLT-AMPS  
VAC - VACUUM  
VDC - VOLTAGE DIRECT CURRENT  
VERT - VERTICAL  
W - WATER  
WTR - WATER  
W - WITH  
W/O - WITHOUT  
WM - WATER MAIN  
WN - WELD NECK  
WNF - WELD NECK FLANGE  
WP - WEATHER PROOF  
WV - WATER VALVE



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## REVISIONS

NO	DESCRIPTION	DATE
1	ADDENDUM 01	07/28/2020

DATE ISSUED: MAY 2020

REVIEWED BY: MRT

DRAWN BY: RCT

DESIGNED BY: RCT

AEP PROJECT NUMBER

201-0251-013

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SHEET TITLE

**INDEX OF  
DRAWINGS AND  
SUMMARY OF  
CONTRACT  
QUANTITIES**

SHEET NUMBER

**C001**

**BID  
DOCUMENTS**





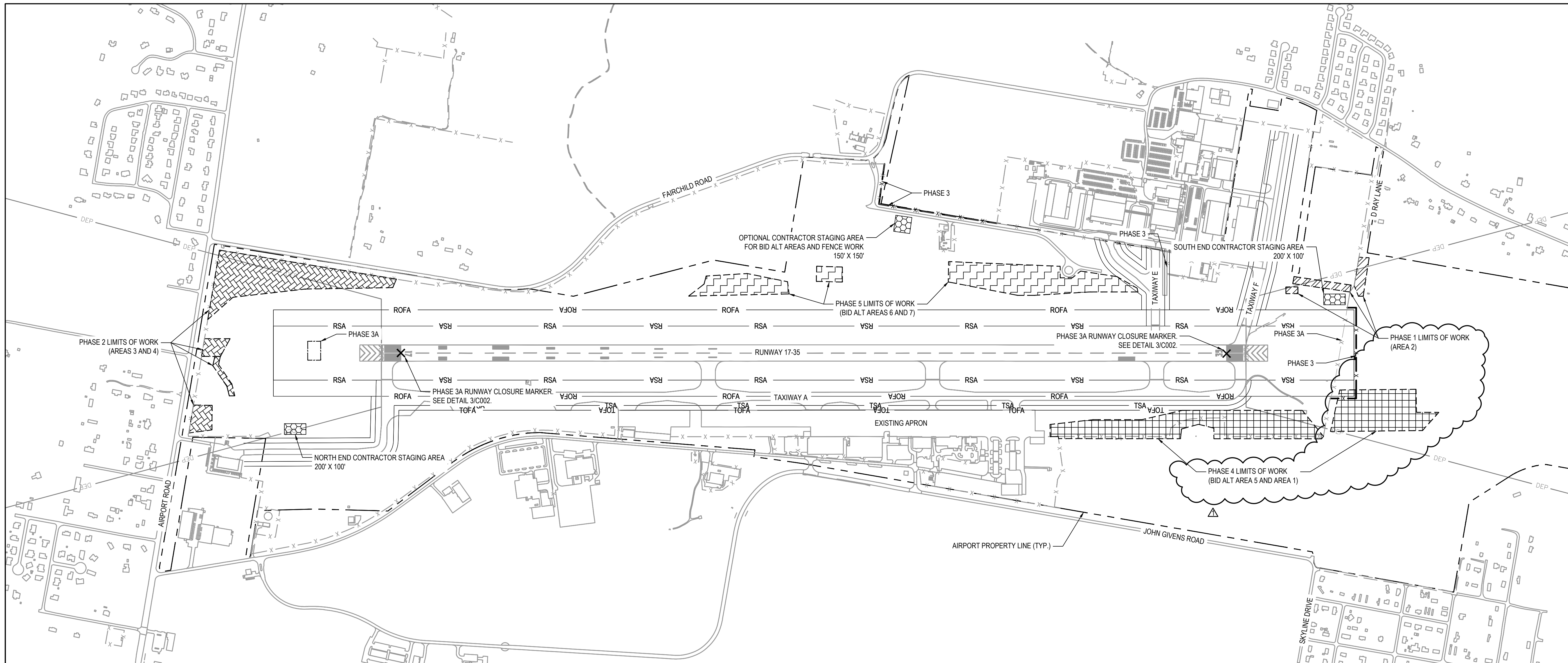
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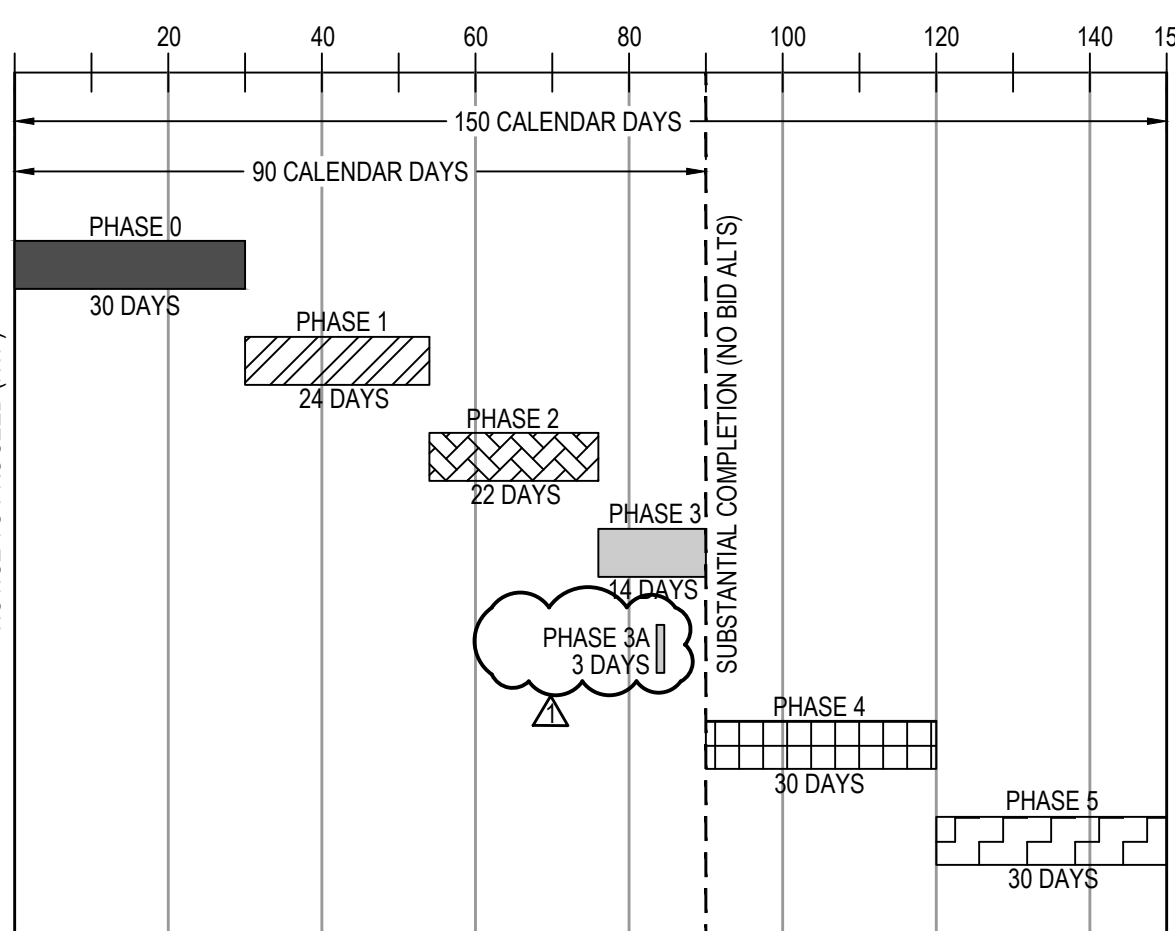


**PHASING NOTES**

- GENERAL**
- THE PHASING SCHEMATIC SHOWN ON THIS SHEET IS INTENDED TO MINIMIZE IMPACTS TO AIRPORT OPERATIONS AND IS SUBJECT TO CHANGE DURING CONSTRUCTION. ALL DURATIONS ARE SHOWN IN CALENDAR DAYS.
  - THE PHASING NOTES HEREIN PROVIDE THE PHASING REQUIREMENTS FOR THE PROJECT. THE CONTRACTOR SHALL SUBMIT TO THE OWNER A DETAILED CONSTRUCTION SCHEDULE MEETING THESE REQUIREMENTS PRIOR TO THE START OF CONSTRUCTION.
  - THERE ARE NO RESTRICTIONS ON CONTRACTOR DAYTIME WORK HOURS FOR THIS PROJECT. NIGHTTIME WORK SHALL BE 10 PM TO 6 AM AND AUTHORIZED ONLY AS SPECIFIED IN THE PLANS OR WHEN OTHERWISE ALLOWED BY THE OWNER.
  - THE CONTRACTOR SHALL, AT ALL TIMES, COORDINATE HIS/HER EFFORTS WITH THE AIRPORT STAFF. IF ANY PROBLEMS ARISE DURING THE CONSTRUCTION SEQUENCING, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE AIRPORT STAFF TO RESOLVE SAID PROBLEMS PRIOR TO CONTINUING THE WORK.
  - THE SECURITY OF THE AIRCRAFT OPERATIONS AREA (AOA) PERIMETER SHALL BE MAINTAINED AT ALL TIMES.
  - MULTIPLE PHASES REQUIRE WORK IN OR NEAR THE RSA, ROFA, TSA, AND TOFA. WHEN WORKING IN THESE AREAS, THE CONTRACTOR SHALL STRICTLY ADHERE TO THE GUIDELINES ESTABLISHED UNDER SAFETY AND SECURITY. THE CONTRACTOR SHALL NOTIFY THE AIRPORT AT MINIMUM 48 HOURS PRIOR TO CONDUCTING WORK IN ANY OF THESE AREAS, AND 14 DAYS PRIOR TO CLOSING ANY AIRPORT PAVEMENTS. SEE CLEARING AND FENCING PLANS FOR OBJECT FREE AREA AND SAFETY AREA IMPACTS ASSOCIATED WITH EACH SPECIFIC WORK AREA.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR DUST CONTROL AND TAKE APPROPRIATE MEASURES AS NECESSARY OR AS DIRECTED BY THE OWNER AND/OR ENGINEER.
  - APPROPRIATE EROSION CONTROL MEASURES AS REQUIRED BY THE CONTRACT DOCUMENTS SHALL BE ACCOMPLISHED PRIOR TO THE BEGINNING OF EACH RESPECTIVE PHASE. REMOVAL OF TEMPORARY EROSION CONTROL SHALL BE ACCOMPLISHED BY THE CONTRACTOR EITHER AT THE COMPLETION OF THE ASSOCIATED PHASE OR THEREAFTER AS DIRECTED BY THE ENGINEER AND/OR OWNER.
  - THE CONTRACTOR SHALL CONTINUOUSLY CLEAN DURING EACH PHASE OF THE PROJECT AND SHALL PERFORM FINAL CLEAN UP WORK PRIOR TO A FINAL INSPECTION.
- PHASE 0 - MOBILIZATION**
- THIS PHASE SHALL CONSIST OF PROJECT MOBILIZATION. MOBILIZATION SHALL INCLUDE DEVELOPMENT OF A COMPREHENSIVE CONSTRUCTION SCHEDULE, PREPARATION AND SUBMISSION OF SHOP DRAWINGS FOR REVIEW AND APPROVAL, PROCUREMENT OF MATERIALS, CONDUCTING PRE-CONSTRUCTION CONFERENCE, ETC.
  - THE CONTRACTOR SHALL HAVE UP TO 30 DAYS FOR THIS PHASE.
- PHASE 1 - AREA 2 - CLEARING AND GRUBBING (1.7 AC)**
- THIS PHASE SHALL CONSIST OF ALL REQUIRED CLEARING AND GRUBBING WITHIN THE LIMITS SHOWN. THIS PHASE CONTAINS LIMITED WORK WITHIN, AND AROUND, THE RUNWAY OBJECT FREE AREA (ROFA). REFER TO SHEET C101.
  - THE CONTRACTOR SHALL HAVE 24 DAYS TO COMPLETE THIS PHASE.

- PHASE 2 - AREAS 3 AND 4 - CLEARING AND GRUBBING (12.3 AC)**
- THIS PHASE SHALL CONSIST OF ALL REQUIRED CLEARING AND GRUBBING WITHIN THE LIMITS SHOWN.
  - THE CONTRACTOR SHALL HAVE 22 DAYS TO COMPLETE THIS PHASE.
- PHASE 3 - FENCING WORK**
- THIS PHASE SHALL CONSIST OF ALL EXISTING SECURITY FENCE REMOVAL AND PROPOSED SECURITY FENCE INSTALLATION. SEE PHASE 3A NOTES FOR RESTRICTIONS.
  - ALL PROPOSED FENCING SHALL BE INSTALLED PRIOR TO, OR IN COORDINATION WITH, THE REMOVAL OF EXISTING FENCE SO AS NOT TO COMPROMISE AOA SECURITY. SEE CLEARING AND FENCING PLANS AND DETAILS.
  - THE CONTRACTOR SHALL HAVE 14 DAYS TO COMPLETE THIS PHASE.
- PHASE 3A - SOUTH END FENCING DEMO AND NORTH END RE-GRADING - NIGHTWORK ONLY**
- THIS PHASE SHALL CONSIST OF REMOVAL OF THE EXISTING SECURITY FENCE LOCATED WITHIN THE RUNWAY SAFETY AREA (RSA) LIMITS ON THE SOUTH END OF THE AIRPORT AND RE-GRADING OF LIMITS WITHIN THE RSA ON THE NORTH END. THIS WORK SHALL BE DONE DURING NIGHTTIME HOURS ONLY AND WILL REQUIRE RUNWAY CLOSURE. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AT A MINIMUM 14 DAYS BEFORE BEGINNING WORK WITHIN THE RSA.
  - THE CONTRACTOR SHALL HAVE 3 CONSECUTIVE NIGHTS TO COMPLETE THIS PHASE. THIS PHASE MAY BE COMPLETED AT ANY TIME DURING PHASE 3, PROVIDED THAT THE ADJACENT PROPOSED FENCING INSTALLATION ALONG THE OUTSIDE OF THE RSA HAS BEEN COMPLETED.
- THE FOLLOWING PHASES ARE APPLICABLE ONLY IF BID ALTERNATIVES ARE ACCEPTED:**
- PHASE 4 - BID ALT AREAS 1 AND 5 - CLEARING AND GRUBBING (18.8 AC)**
- THIS PHASE SHALL CONSIST OF ALL REQUIRED CLEARING AND GRUBBING WITHIN THE LIMITS SHOWN. THIS PHASE CONTAINS LIMITED WORK ADJACENT TO THE TAXIWAY OBJECT FREE AREA (TOFA). SEE CLEARING AND FENCING PLANS.
  - THE CONTRACTOR SHALL HAVE 30 DAYS TO COMPLETE THIS PHASE.
- PHASE 5 - BID ALT AREAS 6 AND 7 - CLEARING AND GRUBBING (12.3 AC)**
- THIS PHASE SHALL CONSIST OF ALL REQUIRED CLEARING AND GRUBBING WITHIN THE LIMITS SHOWN. THIS PHASE CONTAINS LIMITED WORK WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA). SEE CLEARING AND FENCING PLANS.
  - THE CONTRACTOR SHALL HAVE 30 DAYS TO COMPLETE THIS PHASE.

**SCHEMATIC CONSTRUCTION PHASING**



- NOTES:**
- PHASE 3A MAY BE COMPLETED AT ANY TIME DURING THE DURATION OF PHASE 3 BUT MUST BE COORDINATED WITH THE OWNER PRIOR TO COMMENCEMENT.
  - SOME PHASES MAY BE COMPLETED CONCURRENTLY IF REQUESTED BY THE CONTRACTOR AND APPROVED BY THE OWNER IN WRITING.
  - BID ALTS WILL BE ACCEPTED SEPARATELY. ACCEPTANCE OF BID ALT AREA 5 WILL INCREASE THE PROJECT DURATION BY 30 DAYS. ACCEPTANCE OF BID ALTS AREAS 6 OR 7, OR BOTH, WILL INCREASE THE PROJECT TIME BY AN ADDITIONAL 30 DAYS.

**LEGEND**

- PHASE 1 LIMITS
- PHASE 2 LIMITS
- PHASE 4 LIMITS
- PHASE 5 LIMITS
- AIRPORT PROPERTY BOUNDARY
- EXISTING SECURITY FENCE
- PROPOSED SECURITY FENCE
- RSA - RUNWAY SAFETY AREA (RSA)
- ROFA - RUNWAY OBJECT FREE AREA (ROFA)
- TSA - TAXIWAY SAFETY AREA (TSA)
- TOFA - TAXIWAY OBJECT FREE AREA (TOFA)
- 40:1 DEPARTURE SURFACE
- CONTRACTOR STAGING AREA

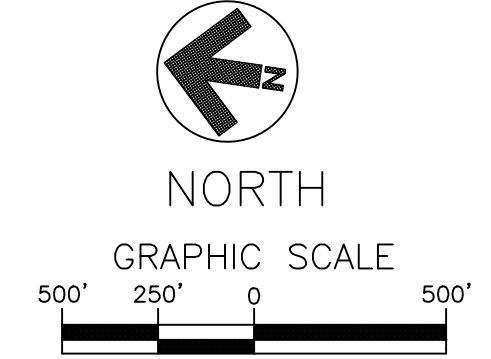
**REVISIONS**

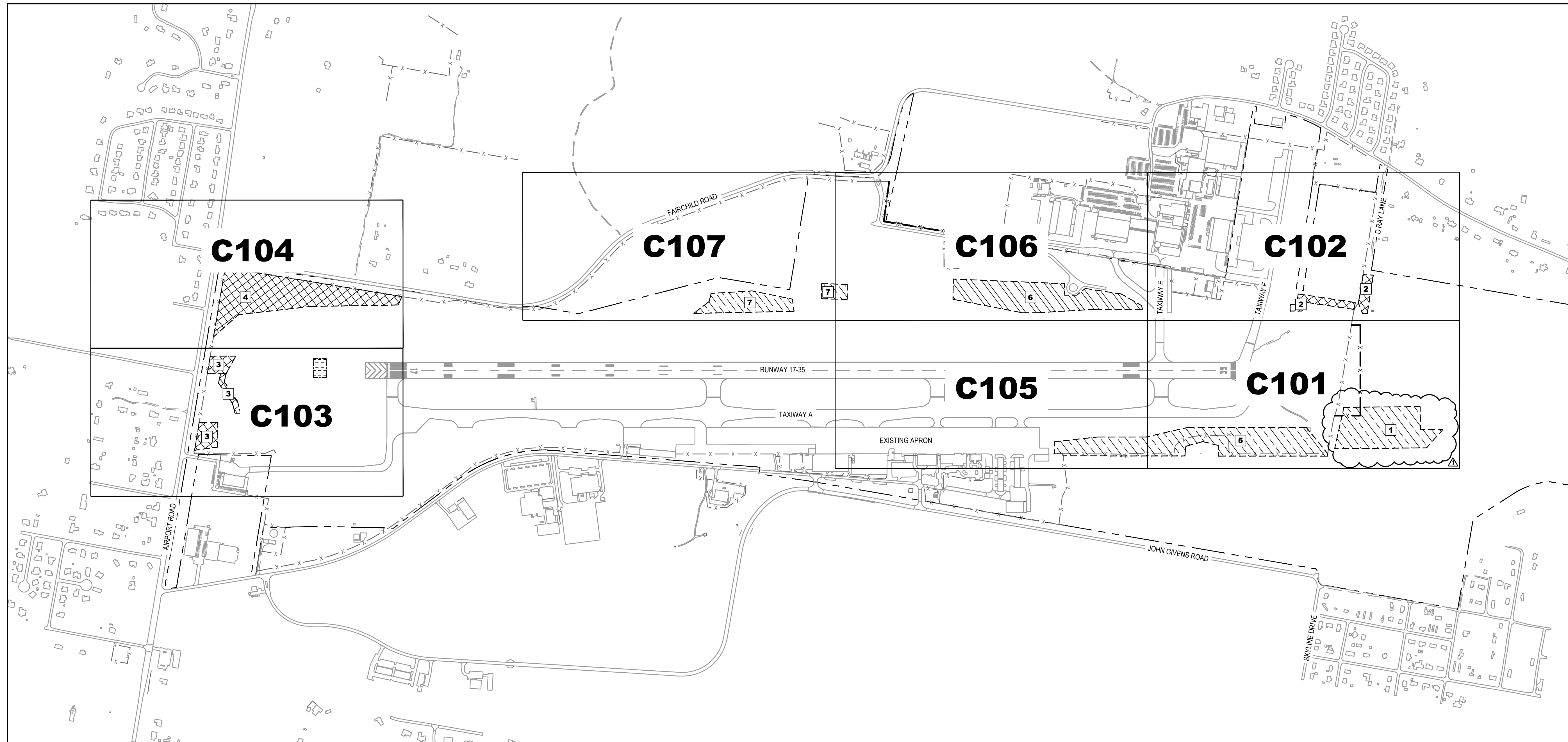
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**CONSTRUCTION  
PHASING PLAN**

SHEET NUMBER  
**C004**  
BID  
DOCUMENTS





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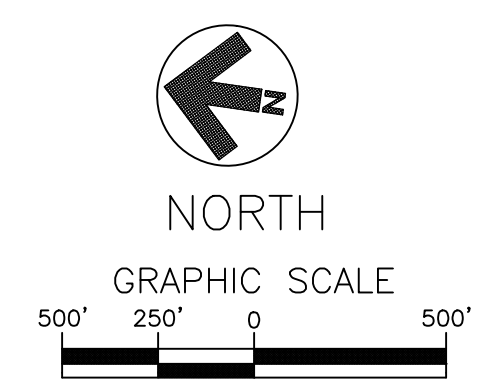
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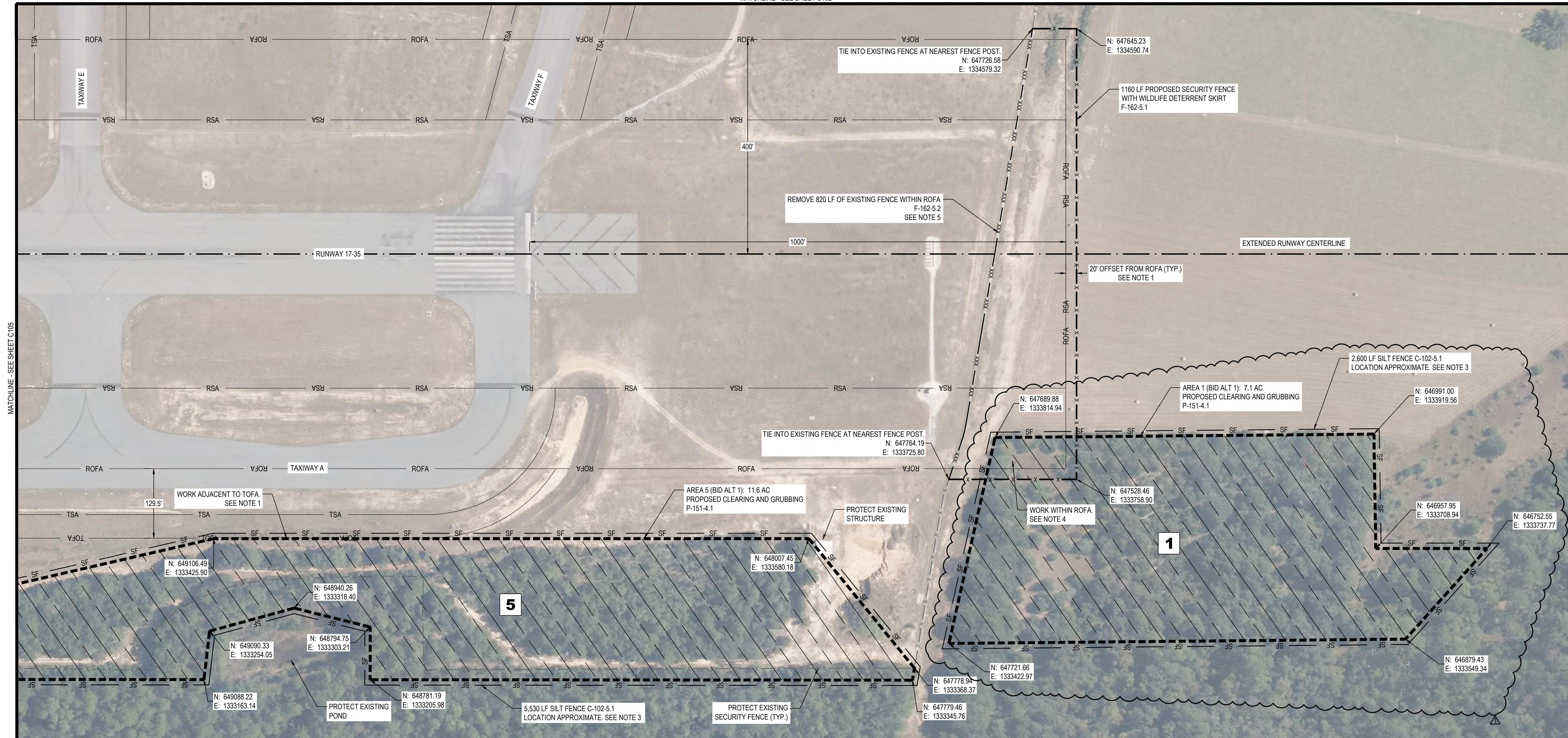
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**CLEARING  
 AND FENCING  
 PLANS -  
 KEY MAP**

SHEET NUMBER  
**C100**  
 BID  
 DOCUMENTS

- LEGEND**
- PROPOSED CLEARING AND GRUBBING LIMITS (BASE BID)
  - PROPOSED CLEARING AND GRUBBING LIMITS (BID ALT 1)
  - PROPOSED CLEARING AND GRUBBING LIMITS (BID ALT 2)
  - PROPOSED CLEARING AND GRUBBING LIMITS (BID ALT 3)
  - PROPOSED RE-GRADING LIMITS (BASE BID)
  - AIRPORT PROPERTY BOUNDARY
  - EXISTING SECURITY FENCE
  - PROPOSED SECURITY FENCE
  - CLEARING AREA DESIGNATION





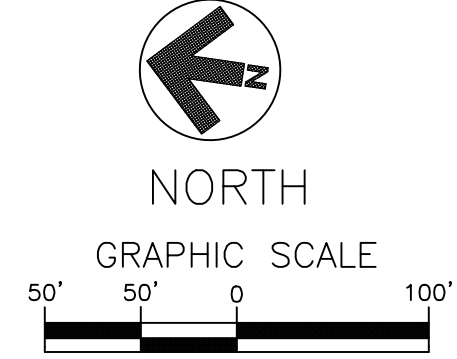
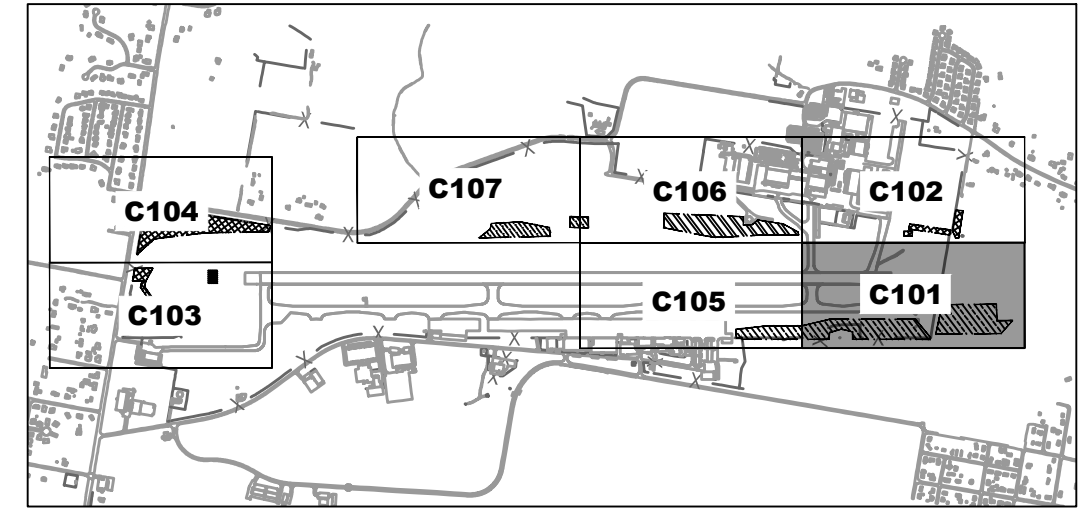
**NOTES**

- CONTRACTOR SHALL STAKE OUT LIMITS OF THE RSA, ROFA, AND TOFA (IF APPLICABLE) IN THE VICINITY OF ALL CLEARING AND FENCING WORK USING THE DIMENSIONS SHOWN. DIMENSIONS ARE FROM RUNWAY AND TAXIWAY CENTERLINES. CONTRACTOR SHALL PLACE TEMPORARY SAFETY BARRIER NETTING AROUND THESE LIMITS UNTIL COMPLETION OF WORK IN EACH AREA AND AVOID ENTERING THESE AREAS UNLESS SPECIFIED BY THE PLANS. SEE SAFETY AND SECURITY DETAILS.
- CONTRACTOR SHALL STAKE OUT AND FLAG THE LIMITS OF CLEARING AND GRUBBING WORK IN EACH AREA USING THE COORDINATES PROVIDED AND RECEIVE APPROVAL FROM THE AIRPORT PRIOR TO WORK COMMENCING IN EACH AREA. CONTRACTOR SHALL TAKE CAUTION NOT TO DISTURB AREAS OUTSIDE THE LIMITS INDICATED.
- CONTRACTOR SHALL INSTALL SILT FENCING AROUND LIMITS OF ALL AREAS TO BE DISTURBED PRIOR TO COMMENCEMENT OF CLEARING IN EACH AREA. SEE EROSION AND SEDIMENTATION CONTROL DETAILS.
- CONTRACTOR SHALL INFORM AIRPORT OPERATIONS A MINIMUM 48 HOURS BEFORE CONDUCTING WORK WITHIN THE ROFA AND/OR TOFA SO THAT PROPER NOTICE TO AIRMEN (NOTAM) MAY BE ISSUED.
- FENCE REMOVAL AND RE-GRADING WITHIN THE RSA REQUIRES CLOSURE OF THE RUNWAY. THIS SHALL BE NIGHTTIME WORK CONDUCTED DURING PHASE 3A ONLY AND REQUIRES NOTICE TO THE AIRPORT A MINIMUM 14 DAYS PRIOR TO WORK COMMENCEMENT. SEE PHASING PLAN AND SAFETY AND SECURITY NOTES AND DETAILS.

**LEGEND**

- PROPOSED CLEARING AND GRUBBING LIMITS (BASE BID)
- PROPOSED CLEARING AND GRUBBING LIMITS (BID ALT 1)
- PROPOSED CLEARING AND GRUBBING LIMITS (BID ALT 2)
- PROPOSED CLEARING AND GRUBBING LIMITS (BID ALT 3)
- PROPOSED RE-GRADING LIMITS (BASE BID)
- AIRPORT PROPERTY BOUNDARY
- EXISTING SECURITY FENCE
- PROPOSED SECURITY FENCE
- SECURITY FENCE TO BE REMOVED
- PROPOSED SILT FENCE
- RUNWAY SAFETY AREA (RSA)
- RUNWAY OBJECT FREE AREA (ROFA)
- TAXIWAY SAFETY AREA (TSA)
- TAXIWAY OBJECT FREE AREA (TOFA)
- RUNWAY CENTERLINE/ EXTENDED CENTERLINE
- CLEARING AREA DESIGNATION

**KEYMAP**



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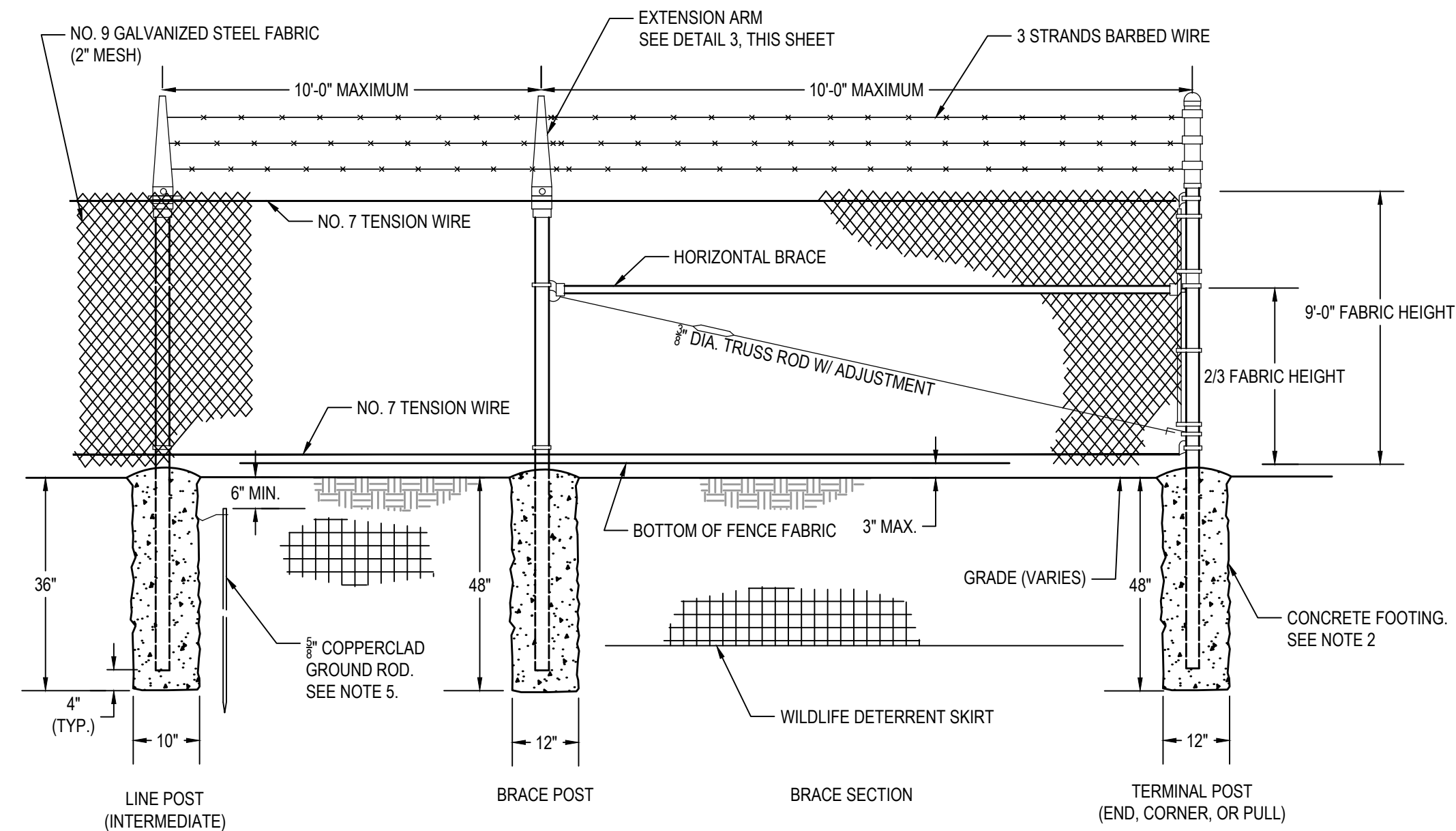
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**CLEARING AND FENCING PLANS (SHEET 1 OF 7)**

SHEET NUMBER  
**C101**  
 BID DOCUMENTS



**1 PERMANENT CHAIN LINK FENCE W/ WILDLIFE SKIRT**  
SCALE: N.T.S.

FENCE MEMBER DIMENSIONS

TYPE	OUTSIDE DIAMETER
CORNER, END, PULL AND BRACE POSTS	2.875"
LINE POSTS	2.875"
HORIZONTAL BRACES	1.66"

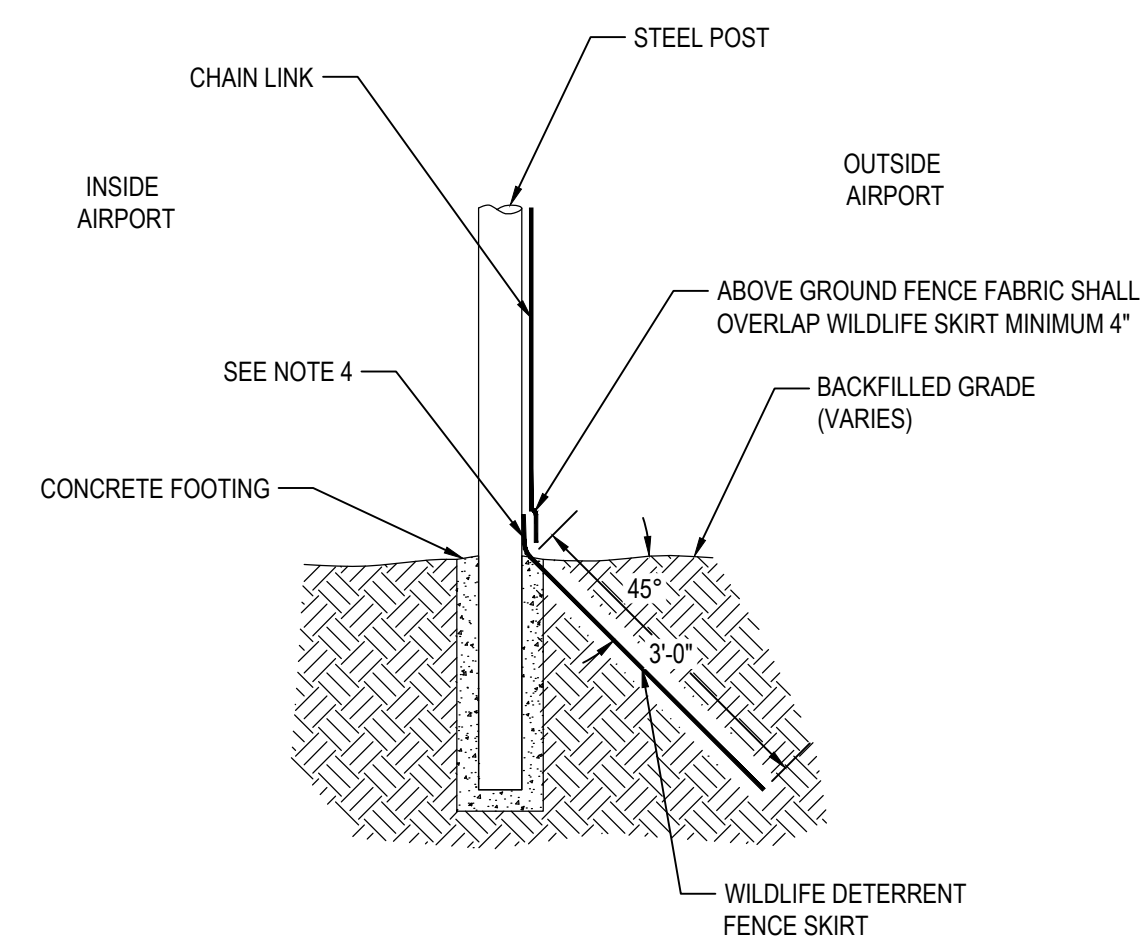
**FENCING NOTES**

FENCE INSTALLATION:

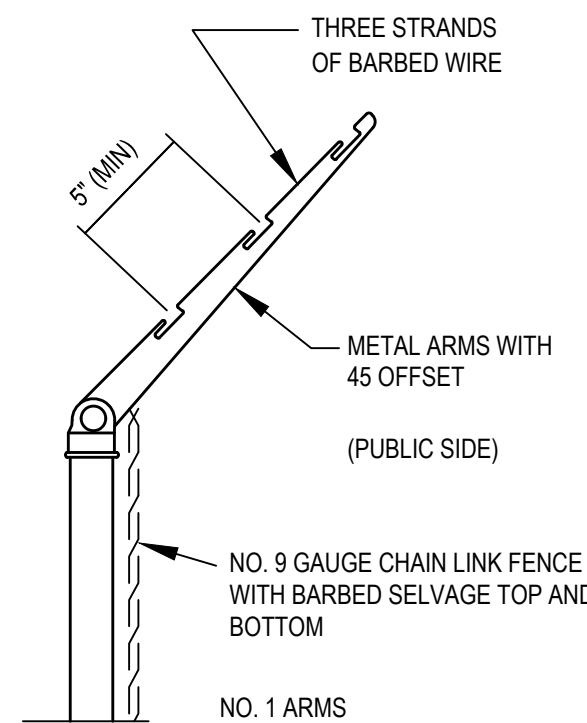
- NOMINAL FOOTING DEPTH IS 36". AT BRACES, GATES, CORNER PULLS, AND ENDS INCREASE FOOTING DEPTH TO 48".
- CONCRETE SHALL MEET MINIMUM STRENGTH REQUIREMENTS OUTLINED IN SPECIFICATION P-610. FOOTING TOP SHALL BE 1" ABOVE GRADE AT THE POST, AND TROWEL FINISHED TO SLOPE AWAY FROM POST. SEE SPECIFICATION P-610 FOR CONCRETE REQUIREMENTS.
- 3" WILDLIFE FENCE SKIRT SHALL ANGLE OUT FROM FINISHED GRADE AT AN ANGLE OF 45 DEGREES. BACKFILL WITH SOIL ON TOP OF SKIRT AFTER INSTALLATION.
- WILDLIFE FENCE SKIRT AND ABOVE GROUND FENCE FABRIC SHALL OVERLAP MINIMUM 4". FASTEN WITH NO. 11 GALVANIZED TIES AT MINIMUM 2'-0" ON CENTER. WILDLIFE FENCE SKIRT SHALL BE INSTALLED WITH ALL PROPOSED FENCING. SEE CLEARING AND FENCING PLANS.
- GROUND RODS SHALL BE INSTALLED AT 50' MAXIMUM INTERVALS, INCIDENTAL TO FENCE COST. GROUND RODS SHALL BE 3/8" BY 8" MIN. SIZE. COPPER CLAD. GROUND CABLE SHALL BE NO. 4 AWG MIN., BARE STRANDED COPPER WIRE.
- ALL FENCING AND FOOTING INSTALLATION ARE INCIDENTAL TO PAY ITEM F-162-5.1. SEE SPECIFICATION F-162 IN THE PROJECT MANUAL FOR MATERIAL REQUIREMENTS.

FENCE REMOVAL:

- FENCE REMOVAL PAY ITEM F-162-5.2 INCLUDES FULL REMOVAL OF ALL FENCING MATERIALS, INCLUDING EXISTING FOOTINGS. AREAS OF REMOVED FOOTINGS SHALL BE BACKFILLED TO EXISTING GRADE. FENCE REMOVED FROM EXISTING PAVEMENT SHALL BE BACKFILLED WITH CONCRETE.
- REMOVED FENCE SHALL BE DISPOSED OF OFF AIRPORT PROPERTY IN A LEGAL MANNER AND SHALL BE INCLUSIVE TO PAY ITEM F-162-5.2.



**2 WILDLIFE DETERRENT FENCE DETAIL**  
SCALE: N.T.S.

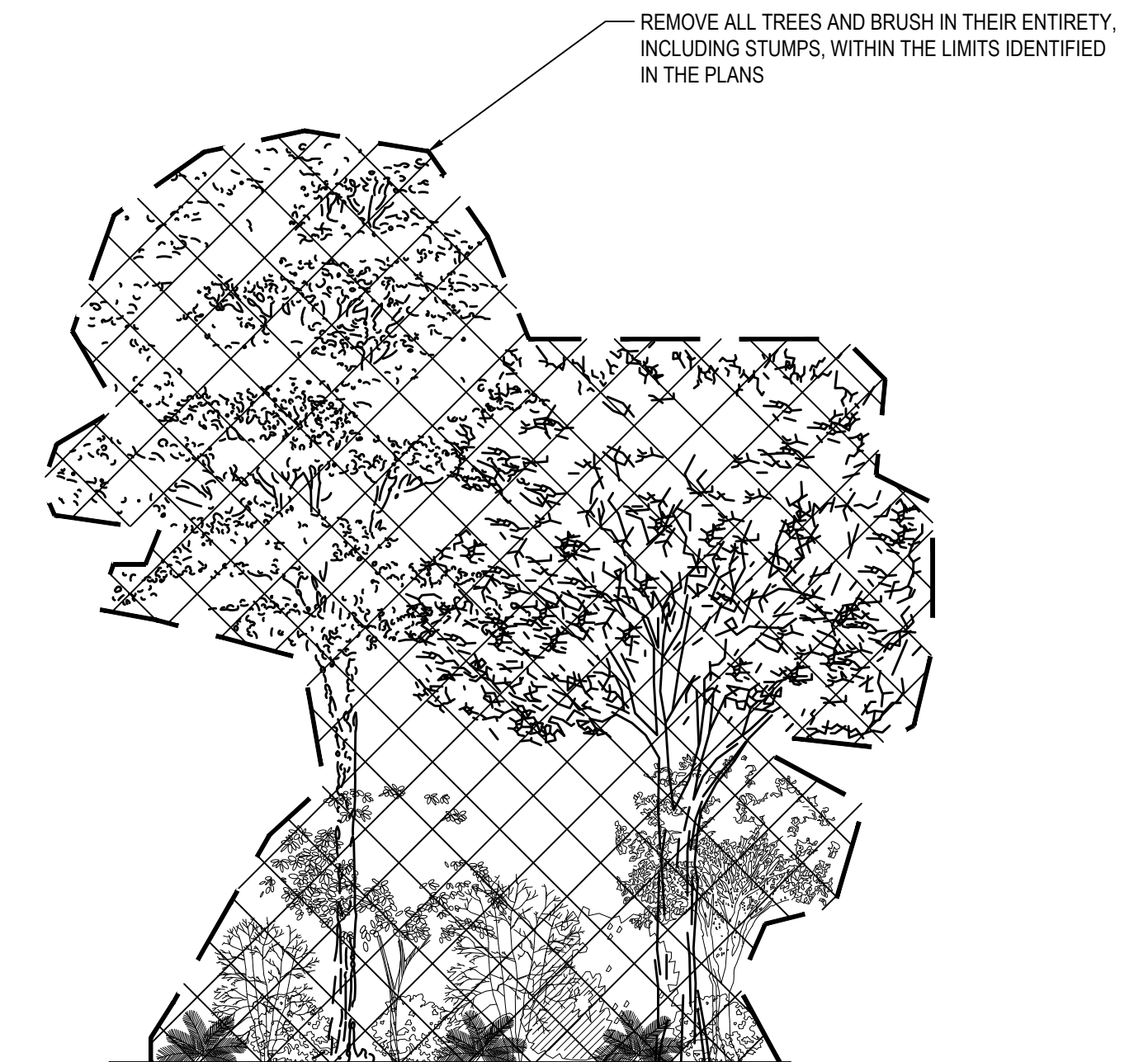


TYPE NO. 1 ARMS SHALL BE INSTALLED ON SIDE AWAY FROM AIRPORT PROPERTY OR AIRCRAFT OPERATIONS AREA.

**3 BARBED WIRE EXTENSION ARMS**  
SCALE: N.T.S.

**CLEARING AND GRUBBING NOTES**

- EXISTING TERRAIN IN AREAS OF CLEARING AND GRUBBING MAY BE UNEVEN AND WET. THE CONTRACTOR SHALL PERFORM THE WORK REQUIRED IN THE PLANS REGARDLESS OF EXISTING SITE CONDITIONS.
- CLEARING AND GRUBBING MAY BE PERFORMED BY MECHANICAL MEANS PROVIDED THAT THE REQUIREMENTS ON THIS SHEET ARE MET.
- AREAS TO BE CLEARED AND GRUBBED SHALL INCLUDE THE COMPLETE REMOVAL OF ALL TREES AND BRUSH WITHIN THE LIMITS IDENTIFIED IN THE PLANS. THIS SHALL INCLUDE THE GRUBBING OF THE STUMPS IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS.
- ALL STUMPS, ROOTS, BURIED LOGS, BRUSH, GRASS, AND OTHER UNSATISFACTORY MATERIALS SHALL BE REMOVED FROM THE INDICATED PROJECT LIMITS. CONTRACTOR MAY DISPOSE OF ALL VEGETATIVE MATERIALS BY LEGALLY DISPOSING OFF AIRPORT PROPERTY OR BY MULCHING IN PLACE AND HAULING TO A LOCATION ON AIRPORT PROPERTY DESIGNATED BY THE OWNER. BURNING OF DEBRIS FROM THIS PROJECT ON AIRPORT PROPERTY IS PROHIBITED. SEE CONTRACT DOCUMENTS.
- UPON COMPLETION OF GRUBBING, CONTRACTOR SHALL REGRADE AND COMPACT THE GROUND SURFACE IN LEVEL SUITABLE CONDITION. THE GROUND SHALL BE SEEDED AND MULCHED IN ACCORDANCE WITH THE SPECIFICATIONS AND IN A MOWABLE CONDITION AT THE COMPLETION OF CONSTRUCTION. GRADING SHALL BE CONSIDERED INCIDENTAL TO THE CLEARING AND GRUBBING PAY ITEM. SEEDING AND MULCHING SHALL BE PAID FOR IN ACCORDANCE WITH THE T-901 PAY ITEMS.
- EXISTING TREES AND OTHER VEGETATION OUTSIDE PROJECT LIMITS TO REMAIN UNDISTURBED SHALL BE PROTECTED FROM DAMAGE THROUGHOUT THE DURATION OF CONSTRUCTION. ANY DAMAGES RESULTING FROM THE CONTRACTOR'S OPERATIONS OR NEGLECT SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.
- ALL HOLES IN EMBANKMENT AREAS REMAINING AFTER THE GRUBBING OPERATION SHALL HAVE THE SIDES OF THE HOLES FLATTENED AND CLEAN FILL SHALL BE USED FILL THE HOLE AND BE COMPACTED TO MATCH SURROUNDING GRADE.



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DOCUMENTS

*OBSTRUCTION CLEARING  
AND PERIMETER FENCING  
IMPROVEMENTS  
CONSTRUCTION  
SAFETY AND PHASING  
PLAN (CSPP)*

*BID DOCUMENTS  
MAY 2020  
REVISED IN ADD 01 –  
JULY 2020*

Bob Sikes Airport  
Okaloosa County  
Crestview, FL



*OBSTRUCTION CLEARING  
AND PERIMETER FENCING  
IMPROVEMENTS  
CONSTRUCTION  
SAFETY AND PHASING  
PLAN (CSPP)*

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Volume No. 1 of 1  
July 2020,  
Crestview, FL

AIP Project No.:  
TBD

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201-0251-013

Prepared by RS&H, Inc. at the  
direction of Okaloosa County  
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## SECTION 1 – PURPOSE

Aviation safety is the primary consideration at airports, especially during construction. The airport operator's Construction Safety and Phasing Plan (CSPP) and the Contractor's Safety Plan Compliance Document (SPCD) are the primary tools to ensure safety compliance when coordinating construction activities with airport operations. These documents identify all aspects of the construction project that pose a potential safety hazard to airport operations and outline respective mitigation procedures for each hazard.

The CSPP sets forth benchmarks and requirements for the project to help ensure the highest levels of safety, security and efficiency at the airport at the time of construction. Guideline requirements for the CSPP are developed from FAA Advisory Circular 150/5370-2G *Operational Safety on Airports During Construction*.

The CSPP is a standalone document, written to correspond with the safety and security requirements set forth in AC 150/5370-2G, the airport safety and security requirements, and local codes and requirements. The CSPP is to be used by all personnel involved in the project. The CSPP covers the actions of not only the construction personnel and equipment, but also the action of inspection personnel and airport staff. This document has been developed in order to minimize interruptions to airport operations, reduce construction costs, and maximize the performance and safety of construction activity. Strict adherence to the provisions of the CSPP by all personnel assigned to or visiting the construction site is mandatory for AIP funded construction projects.

The Contractor shall be required to submit a Safety Plan Compliance Document (SPCD) to the airport operator describing how the Contractor will comply with the requirements set forth in this CSPP. The SPCD must be submitted to the airport operator for approval prior to issuance of the Notice to Proceed. In the event the Contractor's activities are found in non-compliance with the provisions of the CSPP or the SPCD, the Airport Engineer will direct the Contractor, in writing, to immediately cease those operations in violation. In addition, a safety meeting will be conducted for the purpose of reviewing those provisions in the CSPP/SPCD which were violated. The Contractor will not be allowed to resume any construction operations until conclusion of the safety meeting and all corrective actions required by the Contractor have been implemented.

## SECTION 2 – PROJECT SCOPE

Okaloosa County Airports has requested a proposal from RS&H, Inc. (Consultant) to provide design and bidding services for tree clearing and fencing improvements at various locations on the property of the Bob Sikes Airport (CEW). Multiple areas on the airport contain clusters of obstructing trees, identified in the Airport's ALP, that penetrate Departure and Part 77 airspace surfaces. In order to remain compliant with FAA and FDOT regulations, areas that contain obstructing trees will be cleared. In addition, portions of existing perimeter fence that are in current poor condition or require realignment will be replaced.



**FIGURE 1: PROJECT OVERVIEW**

## SECTION 3 – PLAN REQUIREMENTS

### 3.1 COORDINATION

Pre-design, pre-bid, and pre-construction conferences are used to introduce the subject of airport operational safety during construction. In addition, construction progress meetings, scope of schedule changes, and meetings with the airport will be coordinated as required through the performance of the contract.

#### 3.1.1 PRE-DESIGN CONFERENCE

A pre-design kickoff conference was held via conference call with Airport staff. This meeting was used to discuss various items relating to design parameters, airport safety, sequencing of construction operations, environmental considerations, and any other requirements pertinent to the project. This design review conference was essential in identifying and outlining potential affects and/or conflicts to airport operations during construction and ensuring any accommodations can be incorporated into the design documents.

#### 3.1.2 PRE-BID CONFERENCE

The Consultant will conduct a pre-bid conference to help clarify and explain construction methods, procedures, and safety measures required by the contract, prior to the bid opening date. The meeting will discuss items including construction methods, construction procedures (i.e. statistical acceptance testing), operational safety requirements, Disadvantaged Business Enterprise (DBE) and other civil rights and labor requirements.

One of the primary focuses of the Pre-bid Conference is to cover relevant information concerning the Contractor's requirements for developing and submitting an SPCD for review and approval, including both general and specific elements required in the SPCD. In addition, information on how the Contractor shall format the document to illustrate their plans for compliance with those provisions detailed out within this CSPP will also be provided.

Any changes or modifications recommended during the conference will be included in an addendum to the bid documents.

Copies of the proceedings, containing all items discussed, including responses to questions, will be made available to each of the participants, upon request.

#### 3.1.3 PRE-CONSTRUCTION CONFERENCE

The Owner may elect to conduct a pre-construction conference to review specific safety, phasing, and general project requirements and expectations with the Contractor.

#### 3.1.4 SCOPE OR SCHEDULE CHANGES

Changes in the scope and/or duration of the project may necessitate revisions to the CSPP. The FAA Airports Regional or District office shall be promptly notified of any proposed changes to this CSPP.

Changes to this document require review and approval by the airport operator and the FAA prior to implementation. In addition, coordinate proposed changes with any and all appropriate local or federal government agencies (i.e. EPA, OSHA, TSA, state environmental agencies, etc.).

### 3.1.5 FAA ATO COORDINATION

Coordination with FAA ATO will be performed by Airport staff to schedule airway facility shutdowns and restarts. Runway 17-35 is anticipated to be closed for a single night period of the project during work within the Runway Safety Area (RSA). Upon completion of this work, an inspection of the work area should be coordinated well before the beginning of work commences in this phase.

## 3.2 PHASING

Construction phasing for this project will be coordinated with Airport Operations. Phase 3A, which involves a brief closure of the runway, shall be coordinated with Airport Operations, local Air Traffic Control Tower personnel, and airport users. The sequenced construction phases established in this CSPP have been incorporated into the project design and are reflected in the contract drawings and specifications. Phase durations and restrictions are noted in the plans. The Contractor may, with the Owner's written approval, complete work in multiple phases concurrently.

### 3.2.1 PHASE ELEMENTS

The sequence of construction for this project has been phased in order to maintain aircraft operations at an acceptable level of efficiency at the airport for the duration of this contract. General elements of sequencing and phasing are as follows:

- Construction Staging Areas
  - The location of and access to the construction staging area will be as shown on the plans.
- Construction Access and Haul Routes
  - Refer to Contract Layout Plan sheet C003. Applicable control along Contractor haul routes for both safety and security must be maintained at all times. This is especially critical at those locations that require the Contractor to cross or move through active airfield surfaces. Reference Section 3.5.2 VEHICLE AND PEDESTRIAN OPERATIONS, Section 3.16 MARKING AND SIGNS FOR ACCESS ROUTES, and Section 3.18 PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS of this document for additional information. Airport Rescue and Fire Fighting (ARFF) Access Routes—Emergency ARFF access in and around the site will be maintained by the Contractor, as required, for the duration of this project. Contractor must prominently mark open trenches and excavations within the construction site, with approval from Airport Operations and Engineering, and light them with red lights during hours of restricted visibility or darkness.
- Required Hazard Marking and Lighting
  - Reference Section 3.16 MARKING AND SIGNS FOR ACCESS ROUTES, Section 3.17 HAZARD MARKING AND LIGHTING, and Section 3.18 PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS of this document for additional information.
- Lead Times for Required Notifications

- The Contractor is required to coordinate with the Construction Manager and Airport Operations. Lead times for required notifications shall be established at the pre-construction meeting.

Phase specific elements addressed and taken into consideration during the development of the construction phasing for this project are as follows:

- Preconstruction Phase - Mobilization and Submittals
- Phase 1 – Base Bid Tree Clearing Areas on Runway 35 end
- Phase 2 – Base Bid Tree Clearing Areas on Runway 17 end
- Phase 3 – Removal and Installation of Perimeter Fencing
  - Phase 3A – Removal of Existing Fence within the Runway Safety Area (RSA)
- Phase 4 – Bid Alt Tree Clearing Area west of Runway 35 (if awarded by Owner)
- Phase 5 – Bid Alt Tree Clearing Areas east of Runway 17-35 (if awarded by Owner)
- Closeout – Grant Closeout and Demobilization.

### 3.2.2 PRECONSTRUCTION PHASE

- The Contractor will be given 30 days maximum for the Preconstruction Phase.
- The Preconstruction Phase will not include any construction activities other than establishing the Contractor's staging area.
- This Phase is designated for coordination between the Contractor and the Airport for access to and from the staging area and the construction site, as well as other security items such as badging.
- This Phase will also provide time for Contractor submittals to be compiled, submitted and reviewed to potentially avoid delays during construction.

### 3.2.3 PHASE 1

- The following airfield aircraft traffic operations will be modified during this Phase:
  - NOTAM will be issued for work conducted within the Runway Object Free Area (ROFA).
- The contractor must complete Phase 1 within 24 calendar days of commencement.
- Reference the plan for additional safety measures during this phase.
- All work in Phase 1 shall be completed during daytime construction hours.

### 3.2.4 PHASE 2

- This phase is not anticipated to impact airfield aircraft traffic operations.
- All work in Phase 2 shall be completed during daytime construction hours.
- Work within Phase 2 must be completed within 22 calendar days of commencement.

### 3.2.5 PHASE 3

- The following airfield aircraft traffic operations will be modified during this Phase:
  - NOTAM will be issued for work conducted within the Taxiway Object Free Area (TOFA).
- All work in Phase 3 shall be completed during daytime construction hours, with the exception of Phase 3A
- Work within Phase 3 must be completed within 14 calendar days of commencement.

### 3.2.6 PHASE 3A

- The following airfield aircraft traffic operations will be modified during this Phase:
  - Runway 17-35 will be closed. Airport will be closed to all fixed-wing aircraft operations.
- All work in Phase 3A shall be completed during nighttime construction hours

- Work within Phase 3A must be completed within 1 night.
- Taxi routes: No taxiing aircraft at the Airport during this phase.
- Impacts to NAVAIDs: Localizer will be shut down during this phase.
- See Safety and Security Notes and Details as well as Phasing Plan sheets.

#### 3.2.7 PHASE 4

- This phase is not anticipated to impact airfield aircraft traffic operations. Contractor shall take caution not to enter the adjacent Taxiway Object Free Area (TOFA). See plans.
- All work in Phase 4 shall be completed during daytime construction hours.
- Work within Phase 4 must be completed within 30 calendar days of commencement.
- This phase shall be awarded at the Owner's discretion.

#### 3.2.8 PHASE 5

- The following airfield aircraft traffic operations will be modified during this Phase:
  - NOTAM will be issued for work conducted within the Taxiway Object Free Area (TOFA).
- All work in Phase 5 shall be completed during daytime construction hours.
- Work within Phase 5 must be completed within 30 calendar days of commencement.
- This phase shall be awarded at the Owner's discretion.

#### 3.2.9 CLOSEOUT

- The Contractor will be given 15 days maximum for the Closeout Phase.
- The Closeout Phase will not include any construction activities other than demobilizing from the site and restoration of the Contractor's staging area.
- This Phase is designated for Contractor demobilization from the site and the submittal of all required closeout documentation.

### 3.3 AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

Runways, taxiways and other airfield surfaces shall remain in use by aircraft to the maximum extent possible without compromising safety. The performance of this contract will require closures of Runway 17-35 for a brief nighttime duration only.

#### 3.3.1 IDENTIFICATION OF AFFECTED AREAS

See 2.b *Construction Safety Drawings* above for graphical identification of areas affected by construction operations. Of particular concern are the following:

- Closing, of Runway 17-35.
- Closing of Aircraft Rescue and Fire Fighting (ARFF) access routes: The Contractor is required to maintain access in and around the project work area for all ARFF vehicles.
- Closing of access routes used by airport and airline support vehicles: No impacts to airport operations other than the airfield closures listed above.
- Interruption of utilities, including water supplies for firefighting: No utility impacts will be encountered by this project. Work pertaining to existing utilities will be performed without impacting service to Airport.



- Approach/Departure surfaces affected by heights of objects: Equipment shall not exceed 50 feet in height.

Construction areas: These areas include the project work area, storage/stockpile areas, staging areas, and Contractor haul routes near active airfield surfaces. These areas are approximately identified on the Contract Layout Plan. Contractor to confirm staging locations with Airport prior to set up.

### 3.3.2 MITIGATION OF EFFECTS

This CSPP has established specific requirements and operational procedures necessary to maintain the safety and efficiency of airport operations during the construction of this project.

All coordination pertaining to airport operations during construction will go through the Airport Engineer and the Operations Manager. Any required NOTAM's to be issued will be sent through the Airport's Construction Management Representative and issued by Airport Operations.

### 3.3.3 TEMPORARY CHANGES TO RUNWAY AND/OR TAXIWAY OPERATIONS

The affected runway identified in the previous section for reduced access or identified as being closed entirely to aircraft traffic, will have a temporary runway closure sign placed at each runway end as detailed in the Safety and Security details. In addition, required NOTAM's shall be issued on the various temporary changes to aircraft access through the affected areas.

### 3.3.4 DETOURS FOR ARFF AND OTHER AIRPORT VEHICLES

The project work site shall remain open to all ARFF vehicles in emergency situations. The Contractor is required to maintain access in and around the project work area for all ARFF vehicles. Proper routing of this traffic will be effectively communicated to all supervisory personnel involved in the construction project.

### 3.3.5 MAINTENANCE OF ESSENTIAL UTILITIES

Special attention shall be given to preventing unscheduled interruption of utility services and facilities. Where required due to construction purposes, the FAA shall locate all of their underground utilities. The Contractor shall locate and/or arrange for the location of all the underground utilities. When an underground cable or utility is damaged due to the Contractor's negligence the Contractor shall immediately repair the affected cable or utility at his/her own expense. Full coordination between airport staff, field inspectors, and construction personnel will be exercised to ensure that all airport power and control cables are fully protected prior to any excavation. Locations of cabling and other underground utilities will be marked prior to beginning excavation.

### 3.3.6 TEMPORARY CHANGES TO AIR TRAFFIC CONTROL PROCEDURES

Changes to air traffic control procedures must be coordinated with airport ATO.

## 3.4 PROTECTION OF NAVIGATION AIDS (NAVAIDS)

Before commencing construction activity, parking vehicles, or storing construction equipment and materials near a NAVAID, coordination with the appropriate FAA ATO to evaluate the effects of construction activity and the required distances and direction from the NAVAID is required. Construction activities, materials/equipment storage, and vehicle parking near electronic NAVAIDS require special consideration since they may interfere with lines of site and signals essential to air navigation.

### 3.5 CONTRACTOR ACCESS

This CSPP details those areas to which the Contractor must have access, and how Contractor personnel will access those project work areas.

#### 3.5.1 LOCATION OF STOCKPILED CONSTRUCTION MATERIALS

Stockpiled materials and equipment storage are not permitted within the RSA/TSA, OFZ or OFA of an operational runway or taxiway. Stockpiled materials and equipment adjacent to these areas are to be prominently marked and lighted during hours of restricted visibility or darkness. Stockpiled material shall be constrained in a manner to prevent movement resulting from either aircraft jet blast or wind conditions in excess of ten miles per hour. In addition, stockpiled material shall have silt fence located around the material to prevent FOD from moving onto the airfield pavements or polluting watercourses.

Open trenches exceeding 3 inches in depth and 5 inches in width or stockpiled material are not permitted within the limits of safety areas of operational runways or taxiways. Stockpiled material shall not be permitted within the protected areas of the runways or allowed to penetrate any of the protected airspace.

In addition, all demolished pavement materials and demolished fence materials shall be removed and legally disposed of off airport property and not stockpiled on airport property.

All vegetation material from the clearing and grubbing operations must be disposed of. The Contractor may elect to legally dispose of material off Airport property or mulch material in place and haul to a location on Airport property designated by the Owner. Burning of debris from this project on Airport property is prohibited. See Contract Layout Plan for details.

Reference Section 3.7 *Foreign Object Debris (FOD) Management* and Section 3.18 *Protection of Runway and Taxiway Safety Areas* for additional information regarding stockpile management.

#### 3.5.2 VEHICLE AND PEDESTRIAN OPERATIONS

Vehicle and pedestrian access routes for airport construction projects must be controlled to prevent inadvertent or unauthorized entry of persons, vehicles, or animals onto the AOA. The airport operator will coordinate requirements for vehicle operations with the affected airport tenants, Contractors and the FAA air traffic manager. Specific vehicle and pedestrian requirements for this project are as follows:

All construction vehicles and personnel shall be restricted to the immediate work areas specified by the contract for this project. These areas include the haul routes into the work area, the designated Contractor staging area and the areas under construction. Use of alternate haul routes or staging areas by the Contractor shall not be permitted without prior notification and approval by Airport Operations.

Access or haul routes used by Contractor vehicles must be clearly marked to prevent inadvertent entry to areas open to airport operations. Construction traffic must remain on the haul road, never straying from the approved paths. Maintenance and upkeep of the haul roads are the responsibility of the Contractor. Dust must be removed from the haul roads by mechanical sweeping. Application of water on dirt or gravel haul routes must be provided as often as necessary. Haul roads in any airport traffic areas must be especially monitored for dust and debris to prevent any potential Foreign Object Debris (FOD) situations. The Contractor is responsible for any damage caused by construction traffic on the haul roads, regardless of whether in an approved or un-approved traffic area. Following construction

completion, the Contractor shall grade, reseed, clean or otherwise restore the haul route areas to their original conditions prior to construction activities. Special attention must be given to ensure that if construction traffic is to share or cross any ARFF routes that ARFF right of way is not impeded at any time, and that construction traffic on haul roads do not interfere with NAVAIDs or approach surfaces of operational runways. Work necessary in maintaining the haul roads and compliance with safety and security requirements is considered incidental to the project, and therefore, shall not be directly paid for.

Contractor parking and equipment staging areas shall be coordinated with the Airport. Contractor must service all construction vehicles within the limits of the project work area or the Contractor staging area. Parked construction vehicles must be outside the OFZ and never in the safety area of an active runway or taxiway. In some cases, a complex setup procedure makes movement of specialized equipment infeasible (i.e. slip form paving machines and concrete hard forms); inactive equipment must not be parked on closed taxiways or runways. If it is necessary to leave specialized equipment on a closed taxiway or runway at night, the equipment must be well lighted. Employees shall also park construction vehicles outside the OFA when not in use by construction personnel (for example, overnight, on weekends, or during other periods when construction is not active). Parking areas must not obstruct the clear line of sight by the ATCT to any taxiways or runways under air traffic control nor obstruct any runway visual aids, signs, or navigation aids. The FAA must also study those areas to determine effects on airport design criteria, surfaces established by 14 CFR Part 77, Safe, Efficient Use, and Preservation of the Navigable Airspace (Part 77), and on NAVAIDs and Instrument Approach Procedures (IAP).

Vehicles entering the AOA, prior to construction, shall have their tires inspected for FOD. The inspections shall consist of a complete walk around the vehicle to check the tires for FOD and remove any loose materials.

At no time will vehicles or personnel enter portions of the secure AOA outside the contract area unless permitted and accompanied by an airport approved escort.

Operations personnel shall maintain radio communication with air traffic control and monitoring air traffic control frequencies at all times.

All vehicles operating on the airport and in the general vicinity of the safety area or in aircraft movement areas must be marked with flashing yellow/amber beacons or orange and white flags during daylight hours. In addition, the vehicles and equipment will have identifying symbols at a minimum of 8-inch block-type characters of contrasting color that are easily legible. During hours of darkness or low visibility they shall be marked with at least flashing yellow/amber beacons.

Beacons and flags must be maintained to standards and in good working and operational condition. Beacons must be located on the uppermost part of the vehicle structure, visible from any direction, and flash 75 +/- 15 flashes per minute. Flags shall be 3' by 3' with alternating 1' by 1' international orange and white squares and shall be replaced by the Contractor if they become faded, discolored, or ragged as determined by Airport Operations.

No personnel may operate vehicles in the area of operations unless they have first completed and passed an approved driver training class. All personnel operating vehicles on site must attend and complete the airport's driver training course prior to operating vehicles onsite.

At no time shall active taxiways or runways be crossed by construction equipment without notification and proper approval/clearance from Airport Operations and air traffic control.

Aircraft traffic will continue to use existing runways, aprons, and taxiways of the Airport during the time that work under a contract is being performed. The Contractor shall, at all times, conduct the work as to create no hindrance, hazard, or obstacle to aircraft using the Airport.

Airport operators and Contractors must also maintain a high level of security during construction when access points are created in the security fencing to permit construction vehicle access. Temporary gates shall be equipped and/or manned by construction personnel to prevent unauthorized access by vehicles, animals or people. Procedures conforming to Airport security protocols should be in place to ensure that only authorized persons and vehicles have access to the AOA and to prohibit "piggybacking" behind another person or vehicle. Access shall be made available at all times to all airport emergency vehicles traveling to operations areas within the proximity of the construction work zone.

### 3.6 WILDLIFE MANAGEMENT

Construction Contractors must carefully control and continuously remove waste or loose materials that might attract wildlife. Contractor personnel must be aware of and avoid construction activities that can create wildlife hazards on airports.

- **Trash.** Food scraps from construction personnel activity must be collected.
- **Standing water.** Water shall not be allowed to collect and pool for more than any single 24-hour period.
- **Tall grass and seeds.**
- **Poorly maintained fencing and gates.**
- **Disruption of existing wildlife habitat.** Not applicable to this project.

### 3.7 FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

Special care and measures shall be taken to prevent Foreign Object Debris / Damage (FOD) when working in an airport environment. The Contractor shall be held responsible for implementing an approved FOD Management Plan as a part of the SPCD. The FOD Management Plan will have procedures for prevention, regular cleanup, and containment of construction material and debris. The Contractor will ensure all vehicles related to the construction project using paved surfaces in the AOA shall be free of any debris that could create a FOD hazard. Special attention will be given to the cleaning of cracks and pavement joints. All taxiways, aprons, and runways must remain clean. Waste containers with attached lids shall be required on construction sites.

Special attention should be given to securing lightweight construction material (concrete insulating blankets, tarps, insulation, etc.). Specific securing procedures and/or chain-link enclosures may be required.

Contractors will provide their own equipment for vehicle and equipment washing and clean up. All personnel will be responsible for picking up FOD or reporting spills/hazards.

Immediate access to a power sweeper is required when construction occurs on any pavement area inside the AOA, unless an appropriate alternative has been approved by the Airport Engineer and Airport Operations Manager.

### 3.8 HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

Contractors operating construction vehicles and equipment on the airport must be prepared to expeditiously contain and clean-up spills resulting from fuel, hydraulic fluid, or other chemical fluid leaks. Transport and handling of other hazardous materials on an airport also requires special procedures. To that end, the Contractor is required to develop and implement spill prevention and response procedures for vehicle operations. The Contractor shall incorporate these procedures into the SPCD. This includes maintenance of appropriate MSDS data and appropriate prevention and response equipment on-site. Refer to FAA AC 150/5320-15 *Management of Airport Industrial Waste* for more information.

### 3.9 NOTIFICATION OF CONSTRUCTION ACTIVITIES

Following is information and procedures for immediate notification of airport users and the FAA of any conditions adversely affecting the operational safety of the airport.

#### 3.9.1 POINTS OF CONTACT/LIST OF RESPONSIBLE REPRESENTATIVES

Information, Compliance, and Assistance:

(850) 651-7160

Notices to Airmen (NOTAM)

Only the airport operator may initiate or cancel NOTAMs on airport conditions and is the only entity that can close or open a runway or taxiway. The airport operator must coordinate the issuance, maintenance, and cancellation of NOTAMs about airport conditions resulting from construction activities with tenants and the local air traffic facility (control tower, approach control, or air traffic control center), and must provide information on closed or hazardous conditions on airport movement areas to the FAA Flight Service Station (FSS) so it can issue a NOTAM. The airport operator must file and maintain a list of authorized representatives with the FSS. Only the FAA may issue or cancel NOTAMs on shutdown or irregular operation of FAA owned facilities. Any person having reason to believe that a NOTAM is missing, incomplete, or inaccurate must notify the airport operator. See FAA AC 150/5370 2G, Section 3.a.1 regarding issuing NOTAMs for partially closed runways versus runways with displaced thresholds.

Any NOTAMs for planned airfield closures for this project must be coordinated through the airport operations manager and the airports duly appointed construction management representative. Reference Section 3.2 *Phasing* for planned closures for this project, which require issuance of a NOTAM.

#### 3.9.2 EMERGENCY NOTIFICATION PROCEDURES

In the event of an emergency, the Contractor shall be required to Airport Dispatch by calling (850) 651-7160.

In the event of an aircraft emergency, severe weather conditions, or any issue as determined by the Airport that may affect aircraft operations, the Contractor's personnel and/or equipment may be required to immediately vacate the area(s) affected. Points of contact for the various parties involved with the project shall be identified and shared at the pre-construction meeting among the various parties, reference Section 3.1.3 *Pre-construction Conference*. Specific emergency notification procedures shall be incorporated into the Contractor's SPCD.

### 3.10 COORDINATION WITH ARFF PERSONNEL

The Contractor shall coordinate, through the duly appointed airport representative, with ARFF personnel, mutual aid providers, and other emergency services if construction requires the following:

- The deactivation and subsequent reactivation of water lines or fire hydrants, or
- The re-routing, blocking and restoration of emergency access routes, or
- The use of hazardous materials on the airfield.

Procedures and methods for addressing any planned or emergency response actions on the airfield concerning this project shall be established and implemented prior to the start of construction.

### 3.11 NOTIFICATION TO THE FAA

#### 3.11.1 PART 77

Any person proposing construction or alteration of objects that affect navigable airspace, as defined in Part 77, must notify the FAA. This includes construction equipment and proposed parking areas for this equipment (i.e. cranes, graders, other equipment) on airports. FAA Form 7460-1, Notice of Proposed Construction or Alteration, can be used for this purpose and submitted to the appropriated FAA Airports Regional or District Office.

#### 3.11.2 PART 157

With some exceptions, Title 14CFR Part 157, Notice of Construction, Alteration, Activation, and Deactivation of Airports, requires that the airport operator notify the FAA in writing whenever a non-Federally funded project involves the construction of a new airport; the construction, realigning, altering, activating, or abandoning of a runway, landing strip, or associated taxiway; or the deactivation or abandoning of an entire airport. Notification involves submitting FAA Form 7480-1, Notice of Landing Area Proposal, to the nearest FAA Airports Regional or District Office. It is not anticipated that Part 157 notifications will be required for this project.

#### 3.11.3 NAVAIDS

For emergency (short-notice) notification about impacts to both airports owned and FAA owned NAVAIDs, contact: (850) 651-7160, Airport` Operations.

##### 3.11.3.1 Airport Owned/FAA Maintained.

If construction operations require a shutdown of more than 24 hours, or more than 4 hours daily on consecutive days, of a NAVAID owned by the airport but maintained by the FAA, provide a 45-day minimum notice to FAA ATO/Technical Operations prior to facility shutdown.

##### 3.11.3.2 FAA Owned

The airport operator must notify the appropriated FAA ATO Service Area Planning and Requirements (P&R) Group a minimum of 45 days prior to implementing an event that causes impacts to NAVAIDs. (Impacts to FAA equipment covered by a Reimbursable Agreement (RA) do not have to be reported by the airport operator). Coordinate work for an FAA owned NAVAID shutdown with the local FAA ATO/Technical Operations office, including any necessary reimbursable agreements and flight checks. Detail procedures that address unanticipated utility

outages and cable cuts that could impact FAA NAVAIDs. In addition, provide seven days' notice to schedule the actual shutdown.

## 3.12 INSPECTION REQUIREMENTS

### 3.12.1 DAILY (OR MORE FREQUENT) INSPECTIONS

Inspections shall be conducted by the Contractor at least daily, but more frequently if necessary, to ensure conformance with the CSPP. A sample checklist is provided in Reference 1 of this document. In addition to Contractor's required inspections, airport operations will inspect the construction site three (3) times a day to ensure compliance with the CSPP and the SPCD. The Airport may have full-time inspectors monitoring activity throughout construction.

### 3.12.2 FINAL INSPECTIONS

A final inspection with the Airport and FAA will take place prior to close-out of the project.

### 3.12.3 UNDERGROUND UTILITIES

Special attention shall be given to preventing unscheduled interruption of utility services and facilities. Where required due to construction purposes, the FAA shall locate all of their underground cables. The Contractor shall locate and/or arrange for the location of all the underground cables. When an underground cable is damaged due to the Contractor's negligence the Contractor shall immediately repair the cable affected at his/her own expense. Full coordination between airport staff, field inspectors, and construction personnel will be exercised to ensure that all airport power and control cables are fully protected prior to any excavation. Locations of cabling will be marked prior to beginning excavation.

## 3.13 PENALTIES

Failure on the part of the Contractor to adhere to prescribed requirements may have consequences that jeopardize the health, safety or lives of customers and employees at the airport. The Airport may issue warnings on the first offense based upon the circumstances of the incident. Individuals involved in non-compliance violations may be required to be prohibited from working at the airport, pending an investigation of the matter.

Penalties for violations related to airport safety and security procedures will be established by the Airport and/or may be assessed by the FAA, TSA, or a court of competent jurisdiction.

Note: Project shutdown or misdemeanor citations may be issued on a first offense. When construction operations are suspended, activity shall not resume until all deficiencies are rectified.

## 3.14 SPECIAL CONDITIONS

In the event of an aircraft emergency, the Contractor's personnel and/or equipment may be required to immediately vacate the area. The Contractor will receive notification from airport operations when special conditions require the construction site to be vacated. In any event, extreme care should be exercised should construction personnel identify any ARFF (Airport Rescue and Fire-Fighting) vehicle moving toward the Runway with emergency lights displayed. This will generally mean that an emergency situation is imminent.

## 3.15 RUNWAY AND TAXIWAY VISUAL AIDS

Marking, lighting, signs, and visual NAVAIDs. Those areas where aircraft will be operating shall be clearly and visibly separated from construction areas, including closed runways. Throughout the duration of the construction project, the Contractor shall inspect and verify that these areas remain clearly marked and visible at all times and that marking, lighting, signs and visual NAVAIDs remain in place and operational.

#### 3.15.1 GENERAL

Airport markings, lighting, signs, and visual NAVAIDs must be clearly visible to pilots, not misleading, confusing, or deceptive. All must be secured in place to prevent movement by prop wash, jet blast, wing vortices, or other wind currents and constructed of materials that would minimize damage to an aircraft in the event of inadvertent contact.

#### 3.15.2 MARKINGS

Markings are not anticipated to be altered for this project.

#### 3.15.3 LIGHTING AND VISUAL NAVAIDS

All taxiway edge lights in those sections of taxiways closed to aircraft traffic will be either de-energized or blacked out by use of an appropriately cut length of PVC pipe. Centerline lighting that conflicts with the closed taxiway routing shall be either de-energized, removed from the circuit by use of jumpers or as detailed in the project drawing set. Lighting must conform to AC 150/5340-30, *Design and Installation Details for Airport Visual Aids*, AC 150/5345-50, *Specification for Portable Runway and Taxiway Lights*, and AC 150/5345-53, *Airport Lighting Certification Program*.

#### 3.15.4 SIGNS

All taxiway signs in those sections of taxiways closed to aircraft traffic will be either de-energized or blacked out by use of a non-transparent material. Signs are required to conform to AC 150-5345-44, *Specification for Runway and Taxiway Signs*, AC 50/5340-18, *Standards for Airport Sign Systems*, and AC 150/5345-53, *Airport Lighting and Certification Program*.

### 3.16 MARKING AND SIGNS FOR ACCESS ROUTES

Location of haul routes on the airport site shall be as specified in the project drawing set and as provided graphically in the attached exhibits, reference Sheets C003 and C004. It shall be the Contractor's responsibility to coordinate off-site haul routes with the appropriate owner who has jurisdiction over the affected route. The haul routes, to the extent possible, shall be marked and signed in accordance with FAA airfield signage requirements found in AC 150\5340-18, Latest Edition, the Federal Highway Administration Manual on Uniform Traffic Control Devices (MUTCD) and/or state highway specifications. Signs adjacent to areas used by aircraft must meet the airfield general frangibility requirements located in FAA AC 150\5220-23, and as required by the airport and subsequent approval by the Owner. Meeting airfield frangibility requirements may require modification to size and height guidance in the MUTCD.

### 3.17 HAZARD MARKING AND LIGHTING

#### 3.17.1 PURPOSE

Hazard marking and lighting prevents pilots from entering areas closed to aircraft, and prevents construction personnel from entering areas open to aircraft. To that end, comprehensible warning indicators for any area affected by construction that is normally accessible to aircraft, personnel, or



vehicles shall be installed and maintained by the Contractor for the duration of construction operations. Hazard marking and lighting shall also be used for the identification of open manholes, small areas under repair, stockpiled material, waste areas, and taxiway object free areas (TOFA's).

### 3.17.2 EQUIPMENT

Not anticipated for this project.

## 3.18 PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS

Safety area encroachments, improper ground vehicle operations and unmarked or uncovered holes and trenches in the vicinity of aircraft operation surfaces and construction areas are the three most recurring threats to safety during construction. Protection of runway and taxiway safety areas, object free areas, obstacle free zones, and approach/departure surfaces shall be a standing requirement for the duration of construction operations. Reference Section 3.11 *Notification of Construction Activities* and Section 3.15 *Runway and Taxiway Visual Aids* for taxiway closure requirements. Reference Section 3.17 *Hazard Marking and Lighting* for hazard marking. Reference Section 3.18 *Other Limitations on Construction* for height restrictions (as required).

### 3.18.1 TAXIWAY SAFETY AREA (TSA)

The taxiway safety area is a defined surface alongside the taxiway prepared or suitable for reducing the risk of damage to an airplane unintentionally departing the taxiway. No construction may occur within the TSA while the taxiway is open for aircraft operations. Adjustment of TSA dimensions shall be coordinated with the ATCT and the appropriate FAA Airports Regional or District Office; issuing a NOTAM will be required.

Open trenches or excavations are not permitted within the TSA while the taxiway is open. The Contractor must backfill trenches before the taxiway is opened. Coverings are not allowed in taxiway safety areas.

After a taxiway has been closed, Contractors must prominently mark open trenches and excavations at the construction site with red or orange flags, as approved by the airport operator, and light them with red lights during hours of restricted visibility or darkness.

Soil erosion must be controlled to maintain TSA standards, that is, the TSA must be cleared and graded and have no potentially hazardous ruts, humps, depressions, or other surface variations, and capable, under dry conditions, of supporting snow removal equipment, aircraft rescue and firefighting equipment, and the occasional passage of aircraft without causing structural damage to the aircraft.

### 3.18.2 TAXIWAY AND TAXILANE OBJECT FREE AREA (TOFA)

Unlike the Runway Object Free Area, aircraft wings regularly penetrate the taxiway object free area during normal operations. Thus, the restrictions are more stringent than for Runway Object Free Areas. No construction may occur within the TOFA while the taxiway is open for aircraft operations without notice to and permission from Airport Operations.

### 3.18.3 OBSTACLE FREE ZONE (OFZ)

Construction personnel, material, and/or equipment may not penetrate the OFZ while the runway is open for aircraft operations. The OFZ is a defined volume of airspace centered about and above the runway centerline.

#### 3.18.4 RUNWAY APPROACH/DEPARTURE SURFACES

All personnel, materials, and/or equipment must remain clear of the applicable threshold siting surfaces. Objects that do not penetrate these surfaces may still be obstructions to air navigation and may affect standard instrument approach procedures. Coordinate with the FAA through the appropriate FAA Airports Regional or District Office.

Construction activity in a runway approach/departure area may result in the need to partially close a runway or displace the existing runway threshold. Partial runway closure, displacement of the runway threshold, as well as closure of the complete runway and other portions of the movement area also require coordination through the airport operator with the appropriate FAA air traffic manager (FSS if non-towered) and ATO/Technical Operations (for affected NAVAIDS) and airport users.

### 3.19 OTHER LIMITATIONS ON CONSTRUCTION

#### 3.19.1 PROHIBITIONS

The following prohibitions are in effect for the duration of this project:

- No use of equipment with a height of over 50 feet (cranes, concrete pumps, and so on) unless a 7460-1 determination letter is issued for such equipment.
- No use of open flames welding or torches unless fire safety precautions are provided and the airport operator has approved their use.
- No use of electrical blasting caps or explosives of any kind on or within 1,000 ft (300 m) of the airport property.
- No use of flare pots within the AOA.

#### 3.19.2 RESTRICTIONS

- Construction shall not occur inside the Runway Safety Area (RSA) of an active runway at any point in time.